



Social Monitoring Report

Project Number: 47273-004 /005 /006
Loan Numbers: Tranche 2 – 3221 /3222
Tranche 3 – 3325 /3326
Tranche 4 – 3610

Semi Annual Report
June 2019

SRI: Integrated Road Investment Program

— Tranche 2, 3 & 4

PIC 02 – North Western and North Central Provinces

Prepared by the Road Development Authority, Ministry of Highways and Road Development and Petroleum Resources Development for the Asian Development Bank.

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**INTEGRATED ROAD INVESTMENT PROGRAM
NORTH CENTRAL & NORTH WESTERN PROVINCES**

**SEMI ANNUAL SOCIAL COMPLIANCE MONITORING REPORT
JANUARY-JUNE 2019**



REPORT PREPARED BY

**PYUNGHWA ENGINEERING CONSULTANTS In Association with
RESOURCES DEVELOPMENT CONSULTANTS**

On behalf of

Road Development Authority

**Ministry of Highways, Road Development
and Petroleum Resources Development**

Submitted to

Asian Development Bank



In Association with RDC

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Abbreviations	
ADB	Asian Development Bank
ADS	Assistant Divisional Secretary
AIDS	Acquired Immunodeficiency Syndrome
ARE	Assistant Resident Engineer
ASSO	Assistant Social safeguard Officer
CE	Construction Engineer
CSD	Context Sensitive Designs
CSR	Cooperate Social Responsibility
CRCs	Conventional Road Contracts
CO	Colony Officer
DSO	Divisional Secretaries Office
DSDs	Divisional Secretariat Divisions
DSESA	District Social Environmental Safeguard Assitants
DNP	Defects Notification Period
EWCD	Elderly Women Children Disabled
EARF	Environmental Assessment and Review Framework
ESDD	Environmental & Social Development Division(RDA)
ESO	Environmental and Social Officer
ES	Environmental Specialist
FAM	Facility Administrative Manual
GRM	Grievance Redress Mechanism
GAP	Gender Action Plan
GNDs	Grama Niladari Division
GN	Grama Niladari
HIV	Human Immunodeficiency Virus
IPP	Indigenous Peoples Plan
IPPF	Indigenous Peoples Planning Framework
IR	Involuntary Resettlement
i Road	Integrated Road Investment Project
LD	Land Donation
MO	Mahaweli Officer
MOU	Memorandum of Understanding
MFF	Multi Tranche Financing Facility
NCP	North Central Province
NWP	North Western Province
PPTA	Project Preparatory Project Assistance
PE	Project Engineer
PIU	Project Implementation Unit
PD	Project Director
RE	Resident Engineer
RSA	Road Safety Awareness
ROW	Right of Way
RRDE	Rural Road Designe Engineer
SAPE	Survey & Preliminary Engineering Consultancy

SSO	Social Safeguard Officer
SGRS	Social/Gender/Resettlement Specialist
SPS	Safety Policy Statement
SAC	Statement At Completion
TOC	Taking Over Certificate

1.0 Introduction

1.1 Background of the Program

1.1.1 Integrated Road Investment (iRoad) program is mainly focusing on rural roads which are maintained by Provincial Councils and Pradeshiya Saba's. This development effort is very significant since Road Development Authority have taken the responsibility for developing these roads which were belonged to local authorities. Since the population in rural and semi-urban areas are increasing gradually, it is unavoidable that the need of improving linkages with the main cities and city centers to grasp new knowledge and technologies. Still the major part of Sri Lanka's population is concentrated in rural and semi-urban areas. The vital infrastructure facilities in the rural districts have received very little attention over the past years and its rural population, consisting mainly of agrarian folk, is facing immense difficulties due to the very poor condition of the transport infrastructure. This has greatly hindered the spread of economic activities, restricted access to basic health and education resources as well as access to vital markets for procuring basic requirements as well selling their produce.

1.1.2 The Government has addressed this problem by implementing a country wide rural road investment program. The iRoad program which was initiated by the Road Development Department Authority (RDA) under the Ministry of Highways, Road Development and Petroleum Resources Development to improve transport connectivity between rural communities and socioeconomic centers. iRoad program intends to connect 1,000 Grama Niladari Divisions (GNDs) throughout the country as rural hubs and link them to the trunk road network through improving the rural road network to all weather standards and operating a sustainable trunk road network of at least fair condition.

1.1.3 The program, mainly focused on the rehabilitation of existing carriageways to suit all weather conditions. The program is currently under tranche 4 of Multi Tranche Financing Facility (MFF). The selection of rural roads for rehabilitation has been based on the concept of community participation in development and the avoidance of any land acquisition and Involuntary Resettlement (IR). The Resettlement Framework (RF) developed for the program provides guidelines of all social safeguards (including aspects of voluntary land donation) requirements that need to be fulfilled under the program by all social safeguard activities. In line with the guidelines given in the RF, project 4 (NCP) and 5 (NWP) of iRoad program will only develop the roads with in the available Right of Way (ROW). However, if there is a need to take small strips of land from either side of the road for improvements to road safety or drainage, such taking will be done based on the process of "Voluntary Land Donation". The Grievance Redress Mechanism (GRM) has been setup to hear any suggestions, comments or complaints from public regarding the road rehabilitation and improvement works. Transect walks surveys have been conducted during Survey and Preliminary Engineering (SAPE) works as a public participation tool for the four and five projects, especially for road designing, implementing and maintaining. Gender consideration, health, including HIV/AIDS, human trafficking and labour rights, are the other key areas of the social safeguard component of this project in complying with ADB's Safeguard Policy Statement 2009 (SPS).

1.2 The Purpose and the structure of this Report

1.2.1 As the section vii of Resettlement Framework (RF) recommended, semi-annual monitoring reports are to be prepared and submitted by the Project Implementing Consultant (PIC) to RDA, and for further submission to ADB. Complying with contractual requirements SGRS of PIC2 has submitted five semi-annual reports from July 2016 to December 2018. This Semi-Annual Report briefs the progress of social safeguard activities implemented by the Contractors, Project Implementing Consultants (PIC2) and Project Implementing Units (PIUs) of North Central and North Western Provinces from January to June of 2019. First section of this report is the introduction which explains the project background, reserved loan for each package, summery of the progress of each contract package, and the staffing setup of PIU, PIC2 and the Contractors. Section two reports on the progress of civil works during the reporting period, and section three includes the compliance with social safeguard requirements in the loan documents. In the section four of this report explain the status of compliance with the Gender Action Plan. Section five briefs the monitoring of social safeguard compliances at field level while the section six summarizes the issues observed and the status of compliance. Section seven reports the progress in donating lands, and section eight describes coordinating with line agenesis. In the Section nine includes a description of public consultations and information dissemination.

1.3 Staffing Setup at PIU, PIC and Contractor related to Social Safeguard

1.3.1 As mentioned above PIU, PIC and Contractor are the key actors of implementation and monitoring social safeguard activities of iRoad program during the period of project implementation. This report highlights social safeguard performances of two projects, which carried out by these parties and how far two projects have been able to fulfil social safeguard requirements during January to June of 2019, reporting period. Key officials who directly involved in the task are in the table below.

Table No. 1.3.1

Key staff of Social Safeguard activities

PIU	Social Safeguard Officer Assistant Social Safeguard Officer (SSO,ASSO)	From July- 2016 and up to end of the Project in NWP and NCP PIUs
PIC2	Social/Gender/Resettlement Specialist (SGRS)	From June 2016 to June 2021
	Four District Social/Environmental Assistants (DSESA)– RE offices	From Jan-March 2018 to end of June 2019 in NCP and up to 2021 in NWP
	One Environmental/Social Safeguard Officer (ESSO)– TL Office	From February 2017 to End of June 2021
Contractor	Environmental & Social Officer (ESO)	Within the period of end of June 2019 there were 10 ESOs in 15 packages in two projects.(Program activities were almost completed in NCP and KU-01 of NCP,KU-03 and KU-05 have terminated, this reason was affect to vary number of ESOs)

2.0 Progress of Civil Works during the Reporting Period

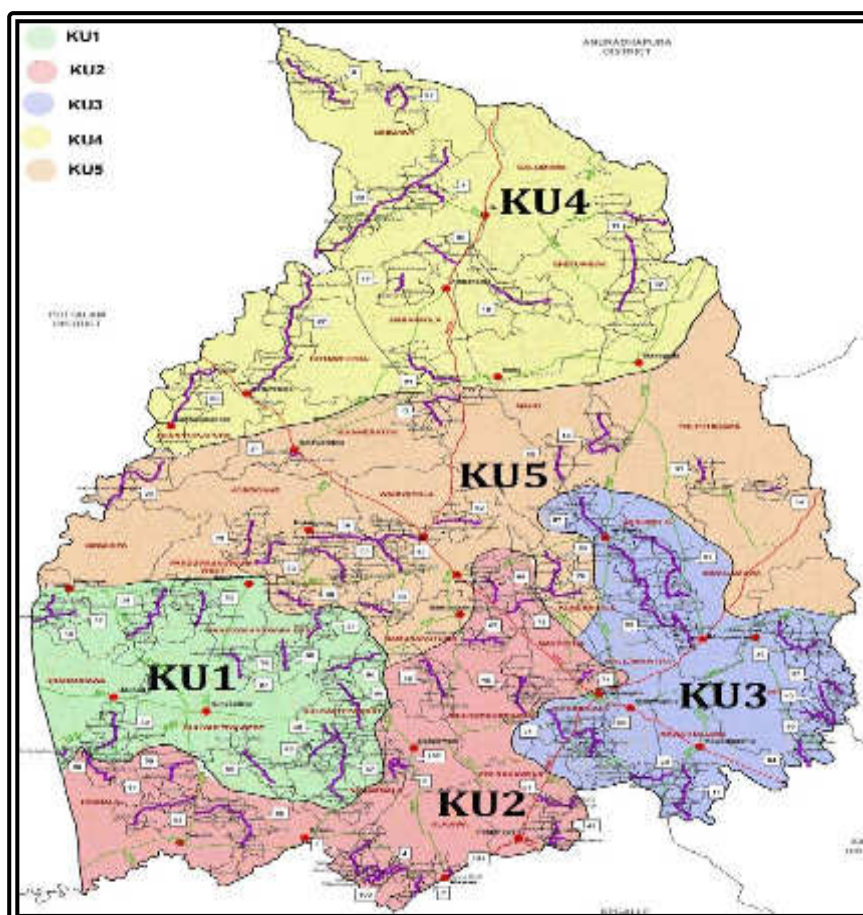
The iROAD Project will upgrade and improve approximately 1201 km of rural access road in the NW & NC Provinces to all weather standards sustaining connectivity between rural communities and socioeconomic centres in Sri Lanka. The roads under the contract packages are isolated lengths of roads dispersed across the North Western Province and North Central Provinces. The PIC Consultant is providing construction supervision services for fifteen (15) CRC's in Anuradhapura, Polonnaruwa, Puttalam and Kurunegala, composed of approximately 1201 km of provincial and rural roads as per the tables below. Initial construction is to be completed over the first 2 years followed by a 3 years' Performance based maintenance period. The Five (5) CRCs in Kurunegala District are listed in Table 2.1

Table 2.1: CRCs in Kurunegala District

No	Contractor	Accepted Contract Amount (LKR)	Cost to completion as at December 2018 (LKR)	Commence ment Date	Completion Date with EOT-1	Completion Date with EOT-2	Completion Date with EOT-3	Road Length (km)/(nos.)
KU1	MAGA Engineering PLC	2,005,790,082.72	1,855,195,883.81	01/08/2016	31/01/2019	NA	NA	83.29/16
KU2	KDAW-BMG JV	2,124,166,266.00	2,074,315,299.90	01/08/2016	29/03/2019	08/05/2019	Under review by Engineer	100.47/19
KU3	Ranken	1,976,721,917.95	1,970,612,536.58	01/08/2016	23/07/2019	Project Terminated. Retendered.		89.88/12
KU4	VVK Construction	2,091,174,540.00	1,917,632,534.62	01/08/2016	30/09/2019	Received notices for EOT	NA	105.98/12
KU5	Ranken	1,996,560,050.43	1,929,219,115.56	01/08/2016	30/07/2019	Project Terminated. Retendered.		99.85/17

KU3 and KU5 contracts had been terminated and retendering is in progress.

The locations of the 479.47 km (76 Nos.) roads in Kurunegala Districts are shown in **Map 2.1** below.



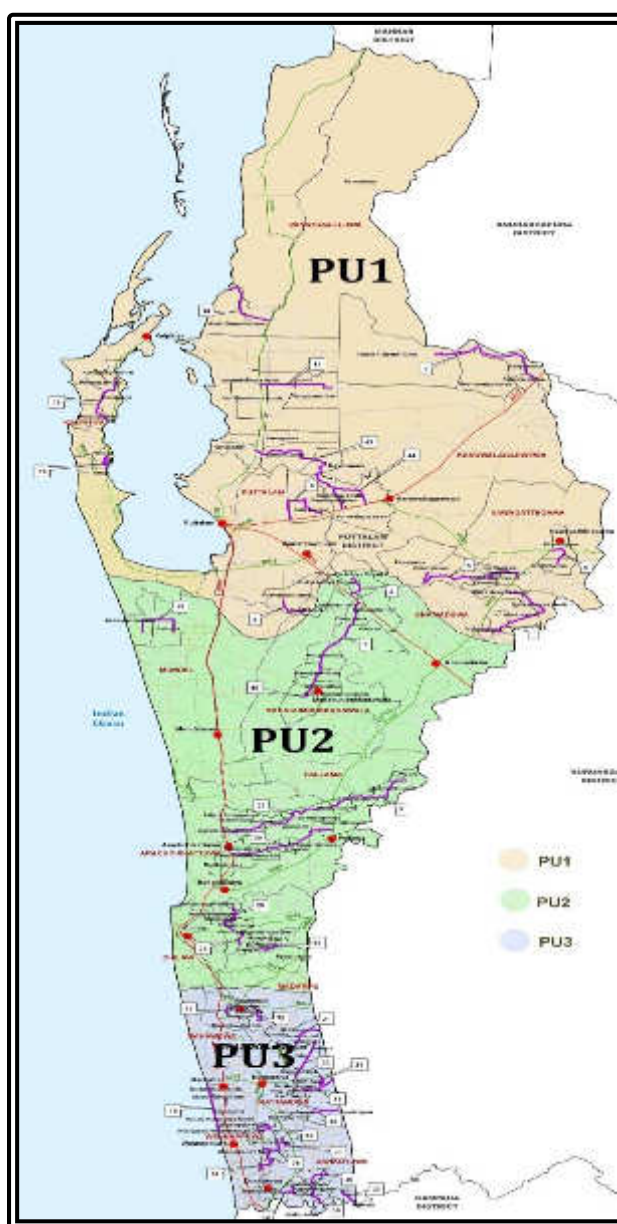
Map 2.1: Locations of 479.32 km (76 Nos.) roads in Kurunegala District

The Three (3) CRCs in Puttalam District are listed in Table 2.2

Table: 2.2: CRCs in Puttalam District

No	Contractor	Accepted Contract Amount (LKR)	Cost to completion as at December 2018 (LKR)	Commence ment Date	Completion Date with EOT-1	Completion Date with EOT-2	Completion Date with EOT-3	Road Length (km)/(nos)
PU1	Ranken	1,706,320,523.61	1,528,656,836.02	01/08/2016	30/04/2019	06/08/2019 Recommended to Employer	Under review by Engineer	90.56/11
PU2	VVK Construction	1,463,917,284.00	1,303,639,714.35	01/08/2016	30/09/2019	NA	NA	74.09/11
PU3	VVK Construction	1,210,355,904.00	850,961,270.16	01/08/2016	31/01/2019	29/05/2019	20/07/2019 Recommended to Employer	48.37/15

The locations of the 213.02 km (37 Nos.) roads in Puttalam Districts are shown in **Map 2.2** below.



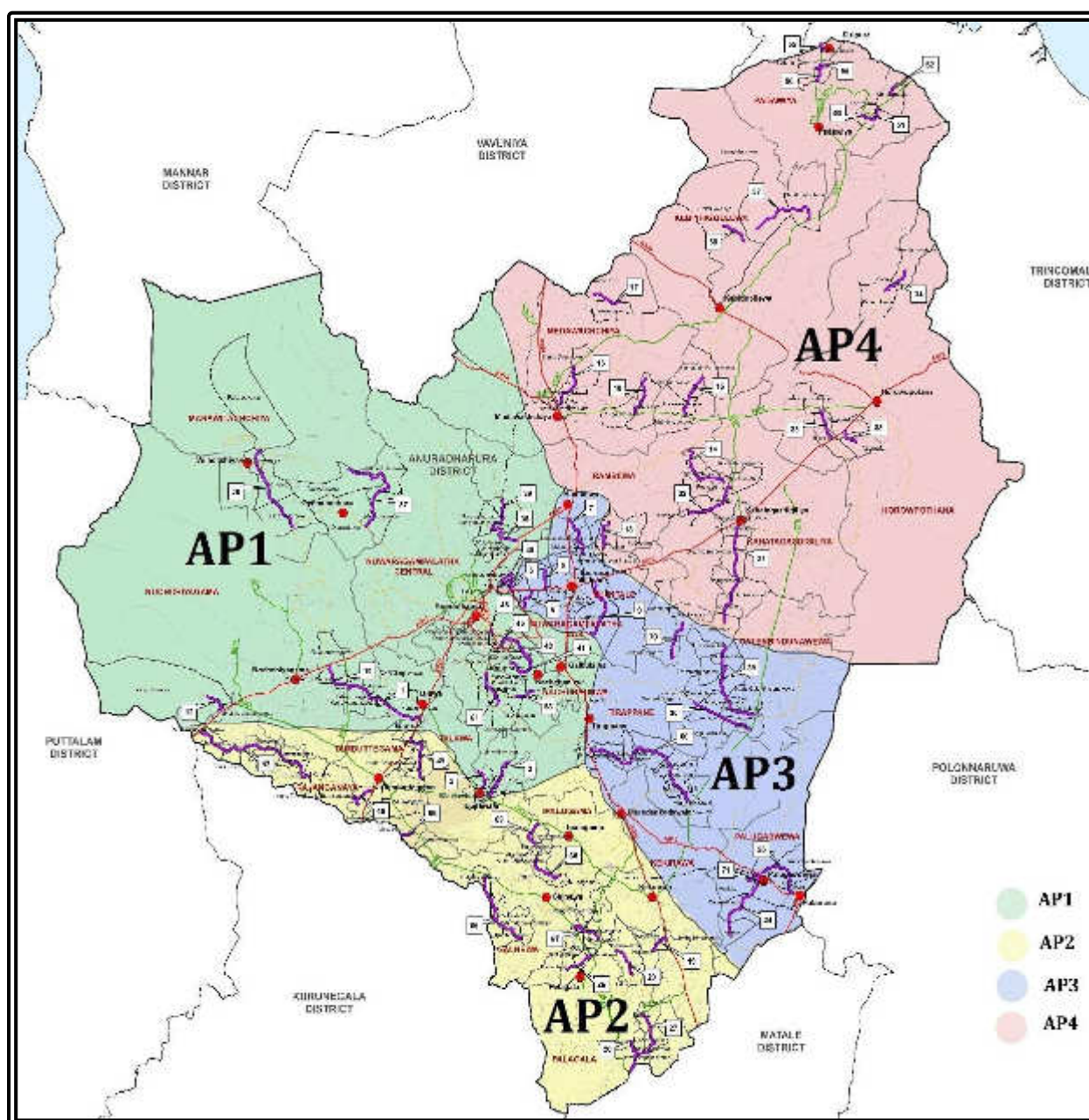
Map 2.2: Locations of 213.02 km (37 Nos.) roads in Puttalam District

The Four (4) CRCs in Anuradhapura District are listed in Table 2.3

Table 2.3: CRCs in Anuradhapura District

No	Contractor	Accepted Contract Amount (LKR)	Cost to completion as at December 2018 (LKR)	Commence ment Date	Completion Date with EOT-1	Completion Date with EOT-2	Road Length (km)/(nos.)
AP1	MAGA Engineering PLC	1,656,915,760.00	1,472,422,749.36	01/08/2016	31/01/2019	30/04/2019 Recommended to Employer	85.39/17
AP2	MAGA Engineering PLC	1,824,178,003.20	1,598,474,135.99	01/08/2016	31/01/2019	NA	76.85/12
AP3	MAGA Engineering PLC	1,492,772,165.28	1,584,486,896.22	01/08/2016	31/01/2019	30/04/2019 Recommended to Employer	82.66/12
AP4	MAGA Engineering PLC	1,635,133,956.00	1,430,172,199.47	01/08/2016	31/12/2018	NA	90.20/19

The locations of the 335.10 km (60 Nos.) roads in Anuradhapura Districts are shown in **Map 2.3** below.



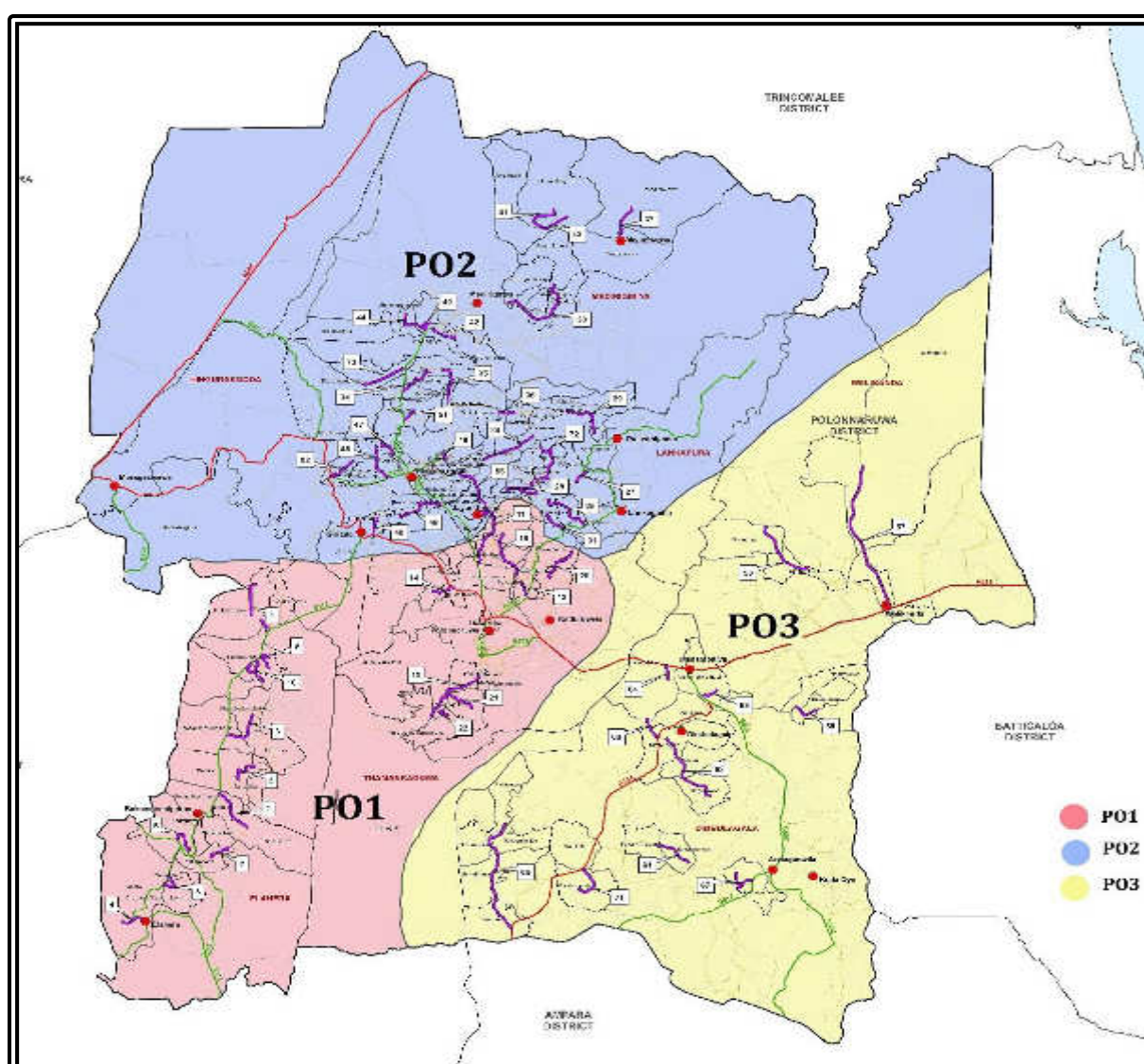
Map 2.3: Locations 335.10 km (60 Nos.) roads in Anuradhapura District

The Three (3) CRCs in Polonnaruwa District are listed in Table 2.4

Table 2.4: CRCs in Polonnaruwa District

No	Contractor	Accepted Contract Amount (LKR)	Cost to completion as at December 2018 (LKR)	Commencement Date	EOT Period	New Completion Date with EOT	Total (km)/(nos.)
PO1	MAGA Engineering PLC	1,158,532,987.42	1,028,656,368.99	01/08/2016	63 Days	02/10/2018	50.76/18
PO2	MAGA Engineering PLC	1,483,692,044.52	1,297,776,614.35	01/08/2016	145 Days	23/12/2018	73.36/25
PO3	Tissa – AMSK Construction	1,249,343,460.00	1,137,136,439.78	01/08/2016	92 Days	31/10/2018	49.55/11

The locations of the 173.67 km (54 Nos.) roads in Polonnaruwa Districts are shown in **Map 2.4** below.



Map 2.4: Locations of 173.67 km (54 Nos.) roads in Polonnaruwa District

2.1. Present Status of CRC Contracts - North Western Province

2.1.1 CRC's in Kurunegala & Puttalam Districts

2.1.1.1 Contract KU1 - Contractor Maga Engineering PLC

The Contract Package KU1 consists of 16 roads totaling 83.29 km and the accepted contract value is LKR 2,005,790,082.00. Please refer to the **Table 2.1** for further details.

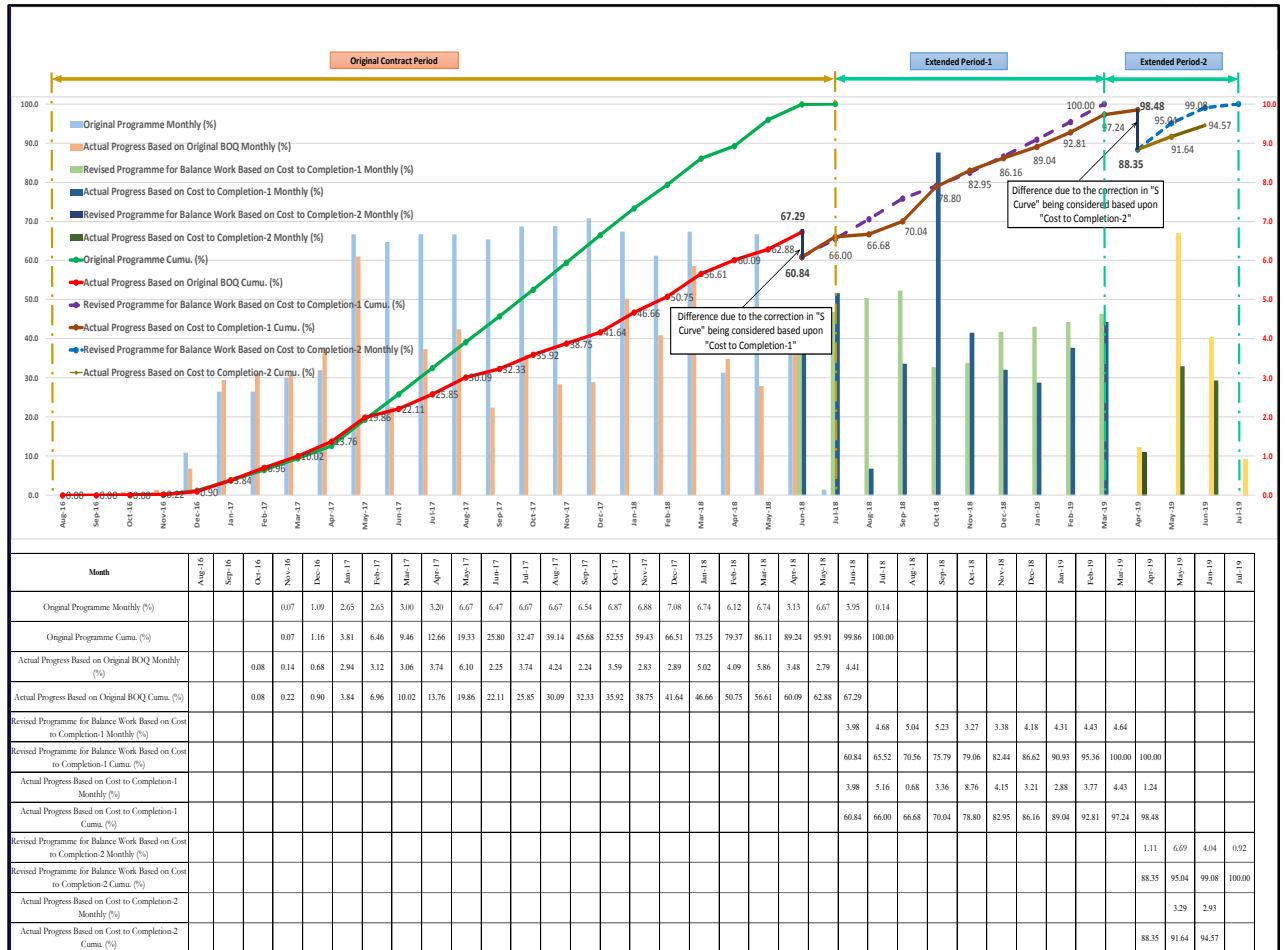
❖ All the major road works were completed in all 16 roads in the contract package by January 2019 and preparation of SAC (Statement At Completion) is 99% completed. Total length is 83.29 Km.

❖ Contract KU1-Roads Handed Over to Client-RDA

	Road No.	Road Name	Road Category	Length/ km
1	30	Talgahapitiya Junction to Walrawa Road	PRDD	5.50
2	32	Kaduruwewa School to Wallawewa School Road	PRDD	2.45
3	34	Meladeniya Junction Udawela Kakiriyamdiththa, Ganegoda to Ambawewa Road	PS	8.23
4	33	Karulla, Horathapola Kibulkotumulla via Paliyagara Road	PS	6.22
5	36	Kmbukgahamulla central coleage Mawatha	PS	1.57
6	76	Kamburapola Junction to Pahalagamuwa, Nindawela Road	PS	4.50
7	97	Hidiyamulla Junction to Guruthippala Junction	PRDD	4.03
8	82	Nagollagoda, Ahetumulla, Higuregama Temple road	PS	4.51
9	81	Mawee ela Junction to Thelibewa Temple via Ilukpitiya Junction	PS	7.50
10	98	Magulagama Junction to Ambagahalanda Mohothhawagoda Junction	PS	4.13
11	47	Bihalpola Muthugala Road	PRDD	10.30
12	48	Ihalamuluthangala to Korale Junction Road	PRDD	3.78
13	49	Nakkawattha, Temple Road, Kaballa Road	PS	2.20
14	50	Katupotha, Dalupothagama, thoranegedara, Kirimatiyawa Road	PRDD	6.62
15	86	Horombawa Mawee Ela Road	PRDD	6.50
16	55	Kosgahamula Junction to Damdeniya Road	PRDD	5.25
Total			-	83.29

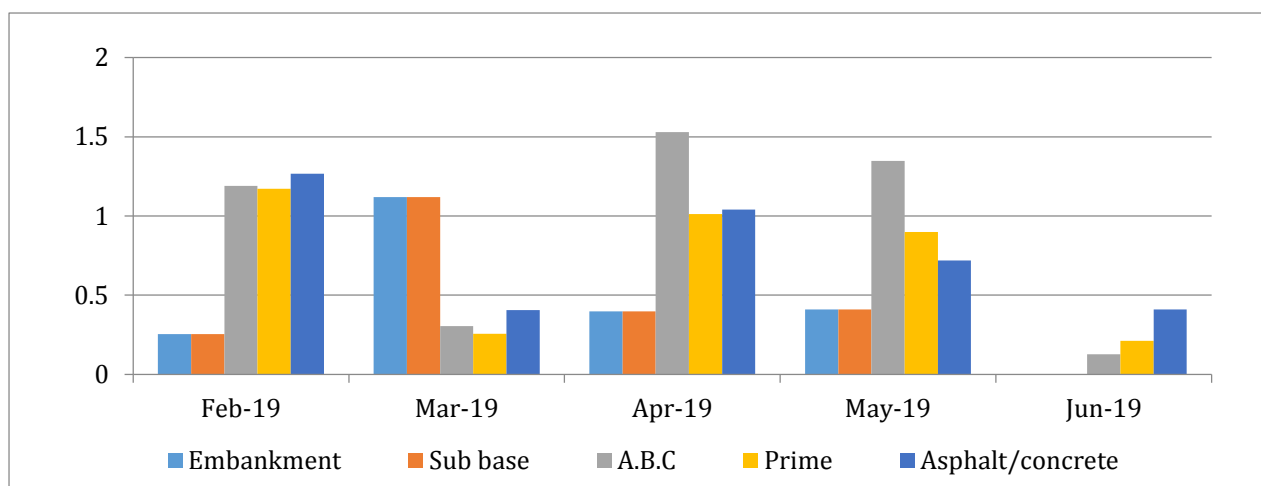
2.1.1.2 Contract KU2 - Contractor KDAW-BMG Joint Venture

The Contract Package KU2 consists of 19 roads totaling 100.47 km and the accepted contract value is LKR 2,124,166,266.00. The Contractor has provided the Engineer's facilities as per the contract with the Construction Engineers office located at Mahameruwatta near Giriulla. The Contractor has established his own office and yard along with his material testing laboratory located at Mahameruwatta all now fully operational. Physical S-Curve & Pre - Construction and Construction Activities shown below:

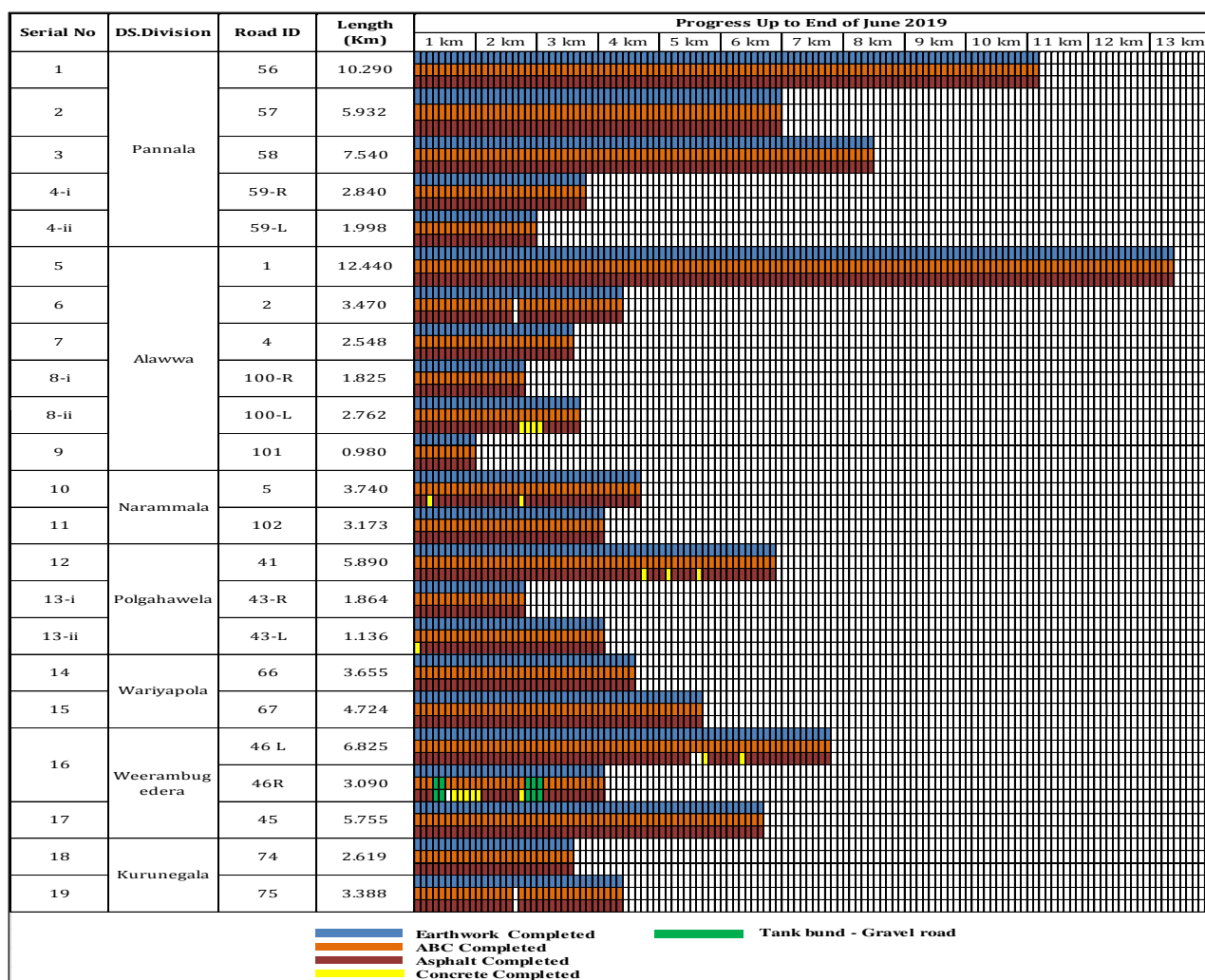


Note: Survey/Investigation & Design Progress are 100% Completed.

KU2 Description							Total (Up to Jun-19)
	Up to Dec-18	Feb-19	Mar-19	Apr-19	May-19	Jun-19	
Embankment	96.665	0.255	1.12	0.398	0.410	0.000	98.848
Sub base	96.665	0.255	1.12	0.398	0.410	0.000	98.848
A.B.C	94.543	1.19	0.305	1.530	1.347	0.127	99.042
Prime	94.542	1.172	0.257	1.012	0.900	0.212	98.095
Asphalt/Concrete (Total Length 99.139 km)	95.061	1.267	0.405	1.040	0.720	0.411	98.904



Road No.	Total Culvert	Culvert Condition Report		New Culvert			Reconstruction Culvert			Redecking Culvert			Widening Culvert									Cleaning & Repairing of Culvert		
	Nos.	Submitted	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	BOTH			LHS			RHS			Total Nos.	Completed	In progress
													Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress			
1	54	48		6	6		6	6					2	2		5	5		2	2		33	33	
2	16	14		2	2		5	5								1	1		1	1		7	7	
4	11	11					4	4					3	3					2	2		2	2	
5	18	14		4	4		3	3					1	1								10	10	
41	50	50					26	26								3	3		4	4		17	17	
43	35	34		1	1		10	10														24	24	
45	20	20					10	10								1	1					9	9	
46	24	23		1	1		8	8					1	1								14	14	
46R	9	9					5	5														4	4	
56	28	24		2	2		1	1								1	1					24	24	
57	18	16		2	2		5	5								1	1		1	1		9	9	
58	22	22					2	2								2	2					18	18	
59	18	18					5	5					4	4					2	2		7	7	
66	15	15					3	3					2	2								10	10	
67	30	28		2	2		3	3					3	3		2	2		3	2		17	17	
74	11	11					1	1								1	1					9	9	
75	15	15					9	9														6	6	
100	39	39					8	8		1	1		2	2		9	9		2	2		17	17	
101	6	6					1	1								1	1					4	4	
102	23	21		2	2		6	6					1	1		2	2		3	3		9	9	
Page Total	462	438	0	22	22	0	121	121	0	1	1	0	19	19	0	29	29	0	20	20	0	250	250	



❖ Contract KU2 - Physical Progress

Progress	Cumulative Status as at 31 st May 2019		Cumulative Status as at 30 th June 2019	
	Target (%)	Target (%)	Target (%)	Actual (%)
Physical	95.04%	91.64%	99.08%	94.57%

- ❖ 16 roads out of 19 have handed over during Jan-June 2019 reporting period.
- ❖ The Contractor has requested for EOT until July 2019 for the following additional works, which were instructed at the latter part of the contract period.
 - (i) R75 - Construction of causeway (27 m)
 - (ii) R66 - Construction of bridge at 3+065 km
 - (iii) R02 - Construction of bridge at 2+700 km
 - (iv) R01 - Construction of drains (300 m)
 - (v) R41 - Construction of retaining wall (82 m), drain construction (50 m), culvert widening at 4+583
 - (vi) R46R - Construction of rigid pavement (390 m)
- ❖ EOT-2 was approved up to 8 May 2019.
- ❖ EOT-3 is under review by the Engineer.

❖ **Contract KU2 - Progress Photographs**



Before construction



Asphalt Construction - Road ID 46R



Before construction



Causeway construction - Road ID 75



Before construction



Form work for rigid pavement construction -
Road ID 46R (1+160 km)

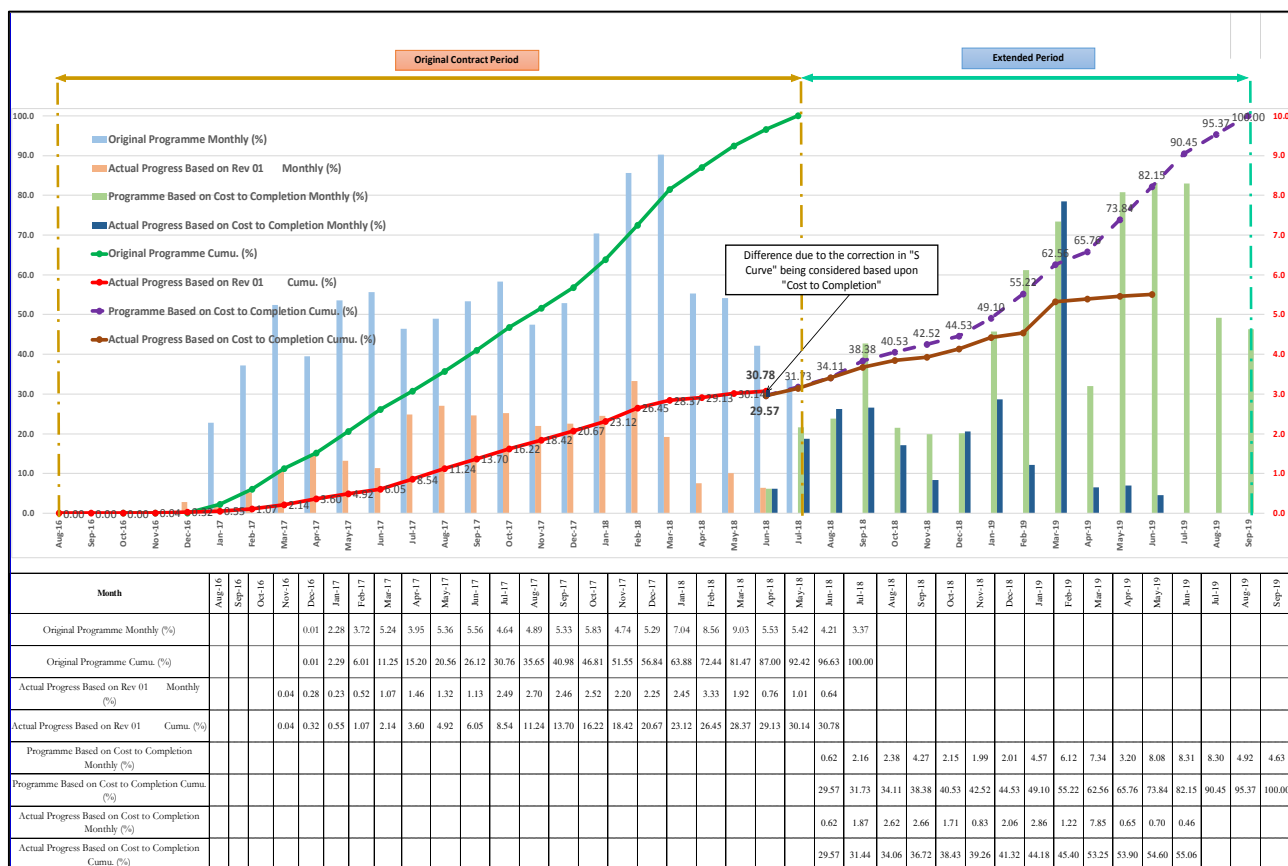
2.1.1.3 Contract KU3 – Contractor Ranken Railway Construction Co. Ltd.

The Contract Package KU3 consists of 12 roads totaling 89.88 km and the accepted contract value is LKR 1,976,721,917.95. As of October 2018 they have abandoned the site and they have closed their site offices with only minor staff. From November 2018 onwards no staff is available on site and the Contractor's site office is still closed. All the machineries from the site has been removed by the Contractor without the consent of the Engineer or the Employer. The Engineer has recommended that the Client exercise their rights in issuing Sub Clause 15.2 – Termination Notice to the Contractor.

This package has been **TERMINATED** through letter Ref. No.: RDA/ADB/iROAD/GEN dated 12.02.2019 and the effective date, by which the Contractor has to vacate the site is on the 26th Feb 2019.

2.1.1.4 Contract KU4 – Contractor V.V. Karunaratne & Company

The Contract Package KU4 consists of 12 roads totaling 105.83 km and the accepted contract value is LKR 2,091,174,540.00. The Contractor has provided the Engineer's facilities as per the contract with the Construction Engineer's office near Ambanpola established and operational. The Contractor has established his own office with yard. His main laboratory building is complete, equipped and operational. Physical S-Curve and Pre-Construction activities shown below:



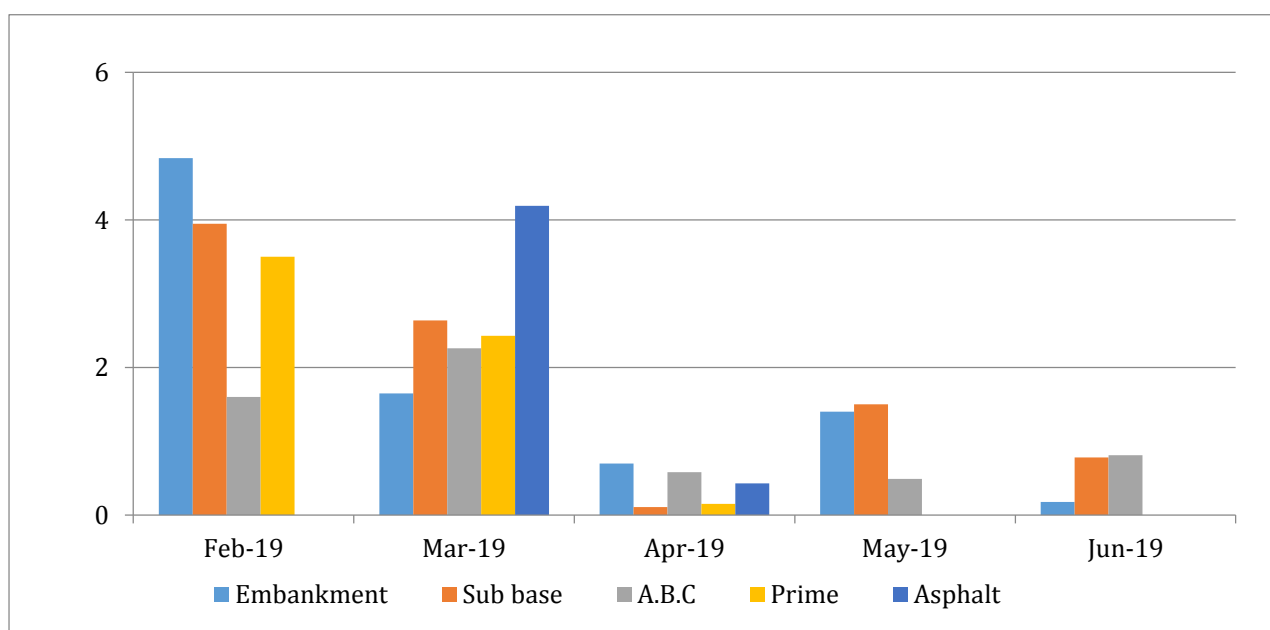
Description	Road No.	Total Length (km)	Horizontal Alignment		Initial Cross section		Investigation			Pavement Design		Vertical Alignment		Design Cross Section	
			Submitted	Approved	Submitted	Approved	Test pit	Lab Testing	Report	Submitted	Approved	Submitted	Approved	Submitted	Approved
	90	18.50	18.50	18.50	18.50	18.50	18.50	18.50	18.50	18.50	18.50	18.50	18.50	15.00	15.00
	8	9.90	9.90	9.90	7.50	7.50	9.90	9.90	9.90	9.90	9.90	7.50	7.50	7.50	7.50
	27	17.47	17.47	17.47	17.47	17.47	17.47	17.47	17.47	17.47	17.47	17.47	17.47	13.57	13.57

Note: Road ID No. 91, 92, 11, 16, 96, 28, 10, 94 & 07 Design & Survey Completed.

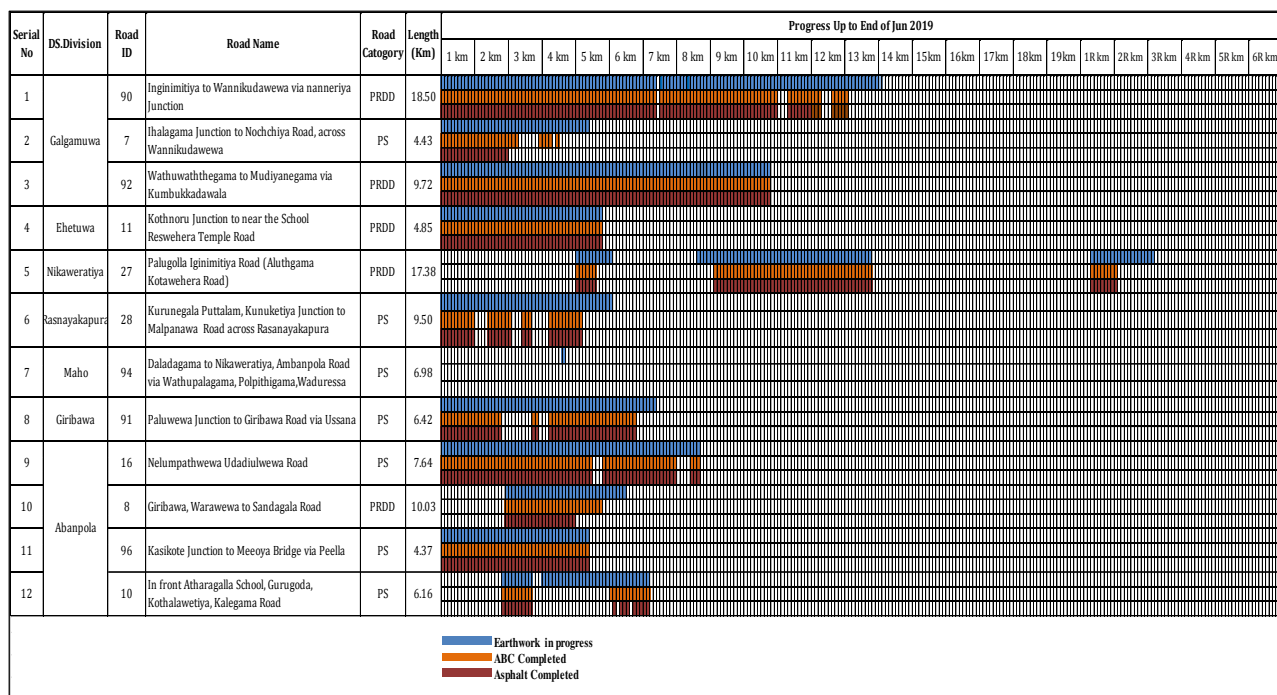
Contractor has submitted Notice of Claims for the following events those were occurred during the EOT-1 period:-

- Adverse weather.
- Suspension of the quarry operations by the Government Authorities.
- New Year holidays.
- Country's security situation.
- Suspension of issuance of explosive material for quarry operations.

KU4 Description							Total (Up to Jun-19)
	Up to Jan-19	Feb-19	Mar-19	Apr-19	May-19	Jun-19	
Embankment	64.53	4.84	1.65	0.70	1.40	0.18	73.30
Sub base	60.34	3.95	2.64	0.11	1.50	0.78	69.32
A.B.C	55.61	1.60	2.26	0.58	0.49	0.81	61.35
Prime	53.02	3.50	2.43	0.15	0.00	0.00	59.10
Asphalt (Total AC Length 101.45 km & tank bund 4.53 km)	50.88	0.00	4.19	0.43	0.00	0.00	55.50



Road No.	Total Culvert	Culvert Condition Report		New Culvert		Reconstruction Culvert			Redecking Culvert			Widening Culvert									Cleaning & Repairing of Culvert			
		Nos.	Submitted	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	LHS			RHS			Both Side			Total Nos.	Completed	In progress		
	Total Nos.										Completed	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress						
7	15	15		1			4	1	1	3	3		1	1		1	1		2	2	0	3		
8	28	28		3	3		2	1					1	1		3	1		1	1		11		
10	18	18		2	2		3	3														11	3	
11	25	25		3	2		3	3					1	1								12	7	
16	27	27		2	2		6	6		1	1		2	2								15	12	
27	45	45					7	7		2	1		1	1		3	3					37	6	
28	48	48					9	3	1	1			2			3			2			34		
90	48	48		5	4	1	9	6														42	7	
91	20	20		1		1	4	4								2	2		3	3		16	7	
92	42	42		1	1		10	10		7	7		1	1		2	2		7	7		23	21	2
94	28	28		1			4						2									25		
96	19	19		2	2								1	1		1	1					9	6	
Page Total	363	363	0	21	16	2	61	44	2	14	12	0	12	8	0	15	10	0	15	13	0	238	69	5



❖ Contract KU4 - Physical Progress

Progress	Cumulative Status as at 31 st May 2019		Cumulative Status as at 30 th Jun 2019	
	Target (%)	Target (%)	Target (%)	Actual (%)
Physical	73.84%	54.60%	82.15%	55.06%

Contract KU4 - Progress Photographs



Before construction



Turf construction - Road ID 92
8+700 km



Before construction



Road Marking - Road ID 96
2+175 km



Before construction



Sub-base construction - Road ID 8
Ch. 5+320 km

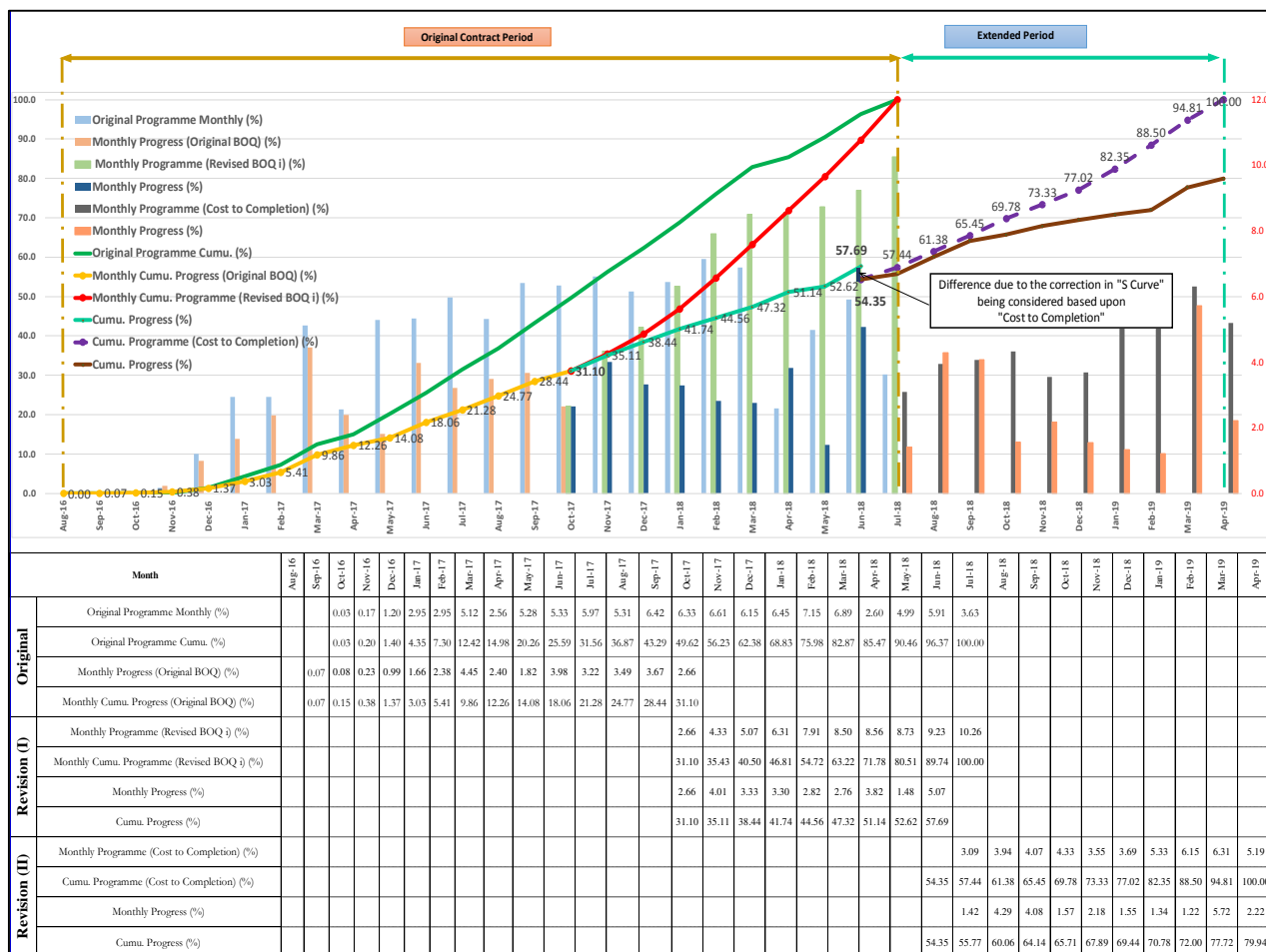
2.1.1.5 Contract KU5 – Contractor Ranken Railway Construction Company Ltd.

The Contract Package KU5 consists of 17 roads totaling 99.85 km and the accepted contract value is LKR 1,996,560,050.43. As of October 2018 they have abandoned the site and they have closed their site offices with only minor staff. From November 2018 onwards no staff is available on site and the Contractor's site office is still closed. All the machineries from the site has been removed by the Contractor without the consent of the Engineer or the Employer. The Engineer has recommended that the Client exercise their rights in issuing Sub Clause 15.2 – Termination Notice to the Contractor.

This package has been **TERMINATED** through letter Ref. No.: RDA/ADB/iROAD/GEN dated 12.02.2019 and the effective date, by which the Contractor has to vacate the site is on the 26th Feb 2019.

2.1.1.6 Contract PU1 – Contractor Ranken Railway Construction Company Ltd

The Contract Package PU1 consists of 11 roads totaling 90.56 km and the accepted contract value is LKR 1,706,320,523.61. The Contractor has provided the Engineer's facilities as per the contract with the Construction Engineers Office in Puttalam now 100 % complete. The Contractor's offices, yard, laboratory and accommodation is also in Puttalam. Contractor's staff mobilization at site is now at a satisfactory level. Physical S-Curve - Pre-Construction activities and construction activities are shown below:



Description	Total Length (km)	Horizontal Alignment		Initial Cross section		Investigation			Pavement Design		Vertical Alignment		Design Cross Section	
Road No.		Submitted	Approved	Submitted	Approved	Test pit	Lab Testing	Report	Submitted	Approved	Submitted	Approved	Submitted	Approved
1	12.11	12.11	12.11	12.11	12.11	12.11	12.11	12.11	12.11	12.11	12.11	12.11	12.11	12.11
12	4.72	4.72	4.72	4.72	4.72	4.72	4.72	4.72	4.72	4.72	4.72	4.72	4.72	4.72

Note: Road ID No. 5, 6, 7, 8, 11, 14, 15, 43, 44 Completed.

Asphalt works completed and incidental works are in progress.

Road No.	Total Culvert	Culvert Condition Report		New Culvert			Reconstruction Culvert			Redecking Culvert			Widening Culvert						Cleaning & Repairing of Culvert		
	Nos.	Submitted	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	LHS			RHS			Total Nos.	Completed	In progress
													Total Nos.	Completed	In progress	Total Nos.	Completed	In progress			
1	45	22		8	8		9	9					3	2		3	2		26		
5	34	16		3	3		3	3					6	5		6	5		10		1
6	11	7		2	2		3	3					1	1		1	1		4		
7	48	8		1	1		5	4								3	3		39		
8	13	7		2	2		3	3								2	2		6	3	
11	16	15		4	3		1	1					3	3		4	4		4		
12	14	11		6	6	0	1	1					3	3		4	4		3		
14	6	6		3	2	0	2	2													
15	11	11		1	1		3	3	0				1	1		1	1		3		
43	37	11		4	3		9	9					5	5		5	5		17	10	
44	11	11		2	1		2	2		1	1	0	1	1		1	1		5	5	
Page Total	246	125	0	36	32	0	41	40	0	1	1	0	23	21	0	30	28	0	117	18	1

Serial No	DS.Division	Road ID	Road Name	Road Category	Length (Km)	Progress Up to End of June 2019													
						1 km	2 km	3 km	4 km	5 km	6 km	7 km	8 km	9 km	10 km	11 km	12 km	13 km	14 km
1	Puttalam	8	Sirambiadiya, 4th Mile Post, Manaweriya, Sellakandal area	PS	7.28														
2	Nawagaththegama	6	Internal Road at Nawagaththegama - Galgamuwa Road	PRDD	2.98														
3		5	Welewewa Garment Junction - Tharanagahawewa - Rajawegama Road	PS	12.01														
4	Karuwalagaswewa	7	Saliyawewa Junction to Neelabemma Road	PRDD	12.95														
5		43	Thabbowa, Karuwalagaswewa, Outer Circle	PRDD	13.07														
6		44	Milankuluma - Thabbowa Temple Junction	PRDD	7.06														
7	Anamaduwa	1	Mahauswewa-Waththegedara-Thalgaswewa	PRDD	12.23														
8	Wanathawilluwa	11	Wanathawillu - Morapothawa School	PS	7.07														
9		12	Eluwankuluma - Gangewadiya Road	PS	4.72														
10	Kalpitiya	14A	Thehili - Aththala Internal Road	PRDD	1.95														
		14B			0.65														
		14C			0.40														
		14D			0.25														
		14E			0.21														
11		15A	Kandakuda - Palliwasalthurei Road	PRDA	5.83														
		15B			1.60														
		15C			0.30														

Note: Earth Work Construction Started for all Roads.

■ Earthwork in progress
■ ABC Completed
■ Asphalt Completed
■ Concrete Pavement

Note : Section from 0+000 to 1+810 km has been deleted from this Contract and will be rehabilitated under another project.

❖ **Contract PU1 - Physical Progress**

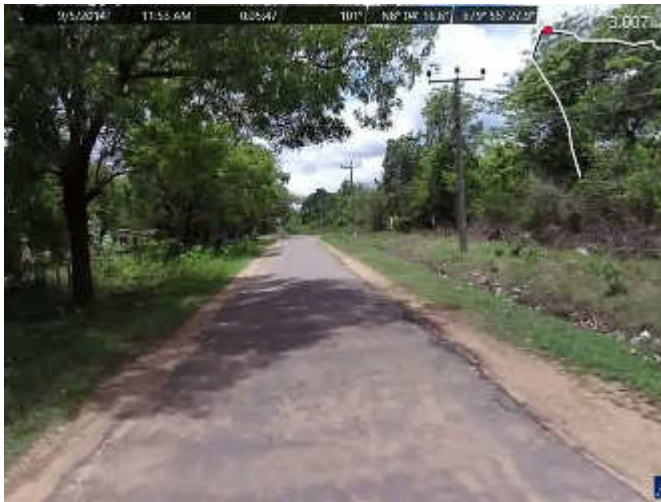
Progress	Cumulative Status as at 31st May 2019		Cumulative Status as at 30th Jun 2019	
	Target (%)	Actual (%)	Target (%)	Actual (%)
Physical	100.00%	82.13%	100.00%	94.72%

Note:- CTC as of December 2018 was Rs 1,528 Mn, it has been reduced to Rs 1,348 Mn as of June 2019. Based on the revised CTC the physical progress was recalculated.

- Engineer has recommended for EOT-2 up to 06 August 2019 and resubmitted the documents to Employer on 26 June 2019.
- EOT-3 is under review by the Engineer.

The Contract package was not able to hand over any road during reporting period.

❖ **Contract PU1 - Progress Photographs**



Before Construction



After the erection of sign boards at Road ID 44
3+007 km



Before Construction



After the construction of drain at Road ID 05
0+105 km



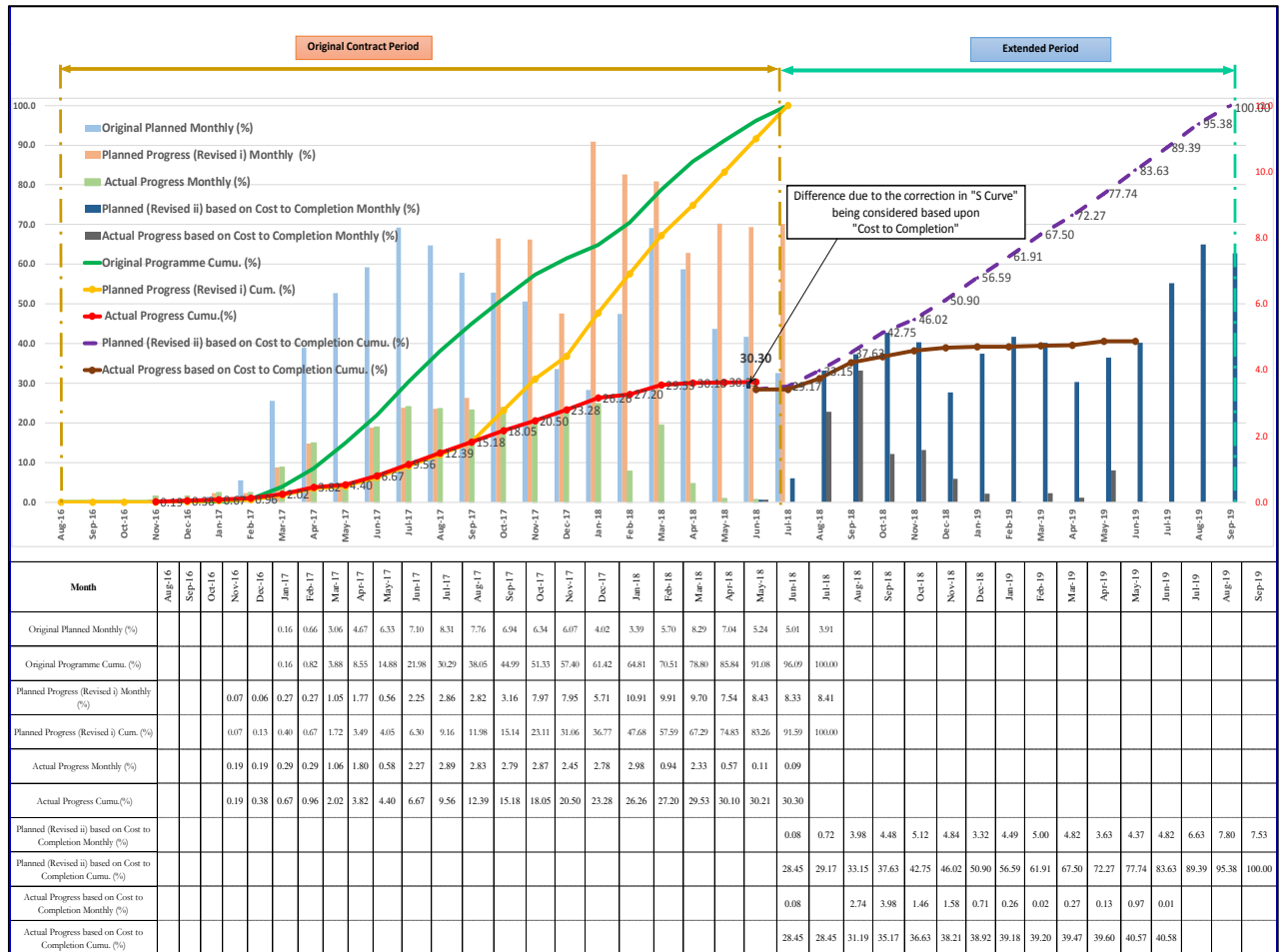
Before Construction



After the erection of signboards at Road ID 08
3+645 km

2.1.1.7 Contract PU2 – Contractor V. V. Karunaratne& Company

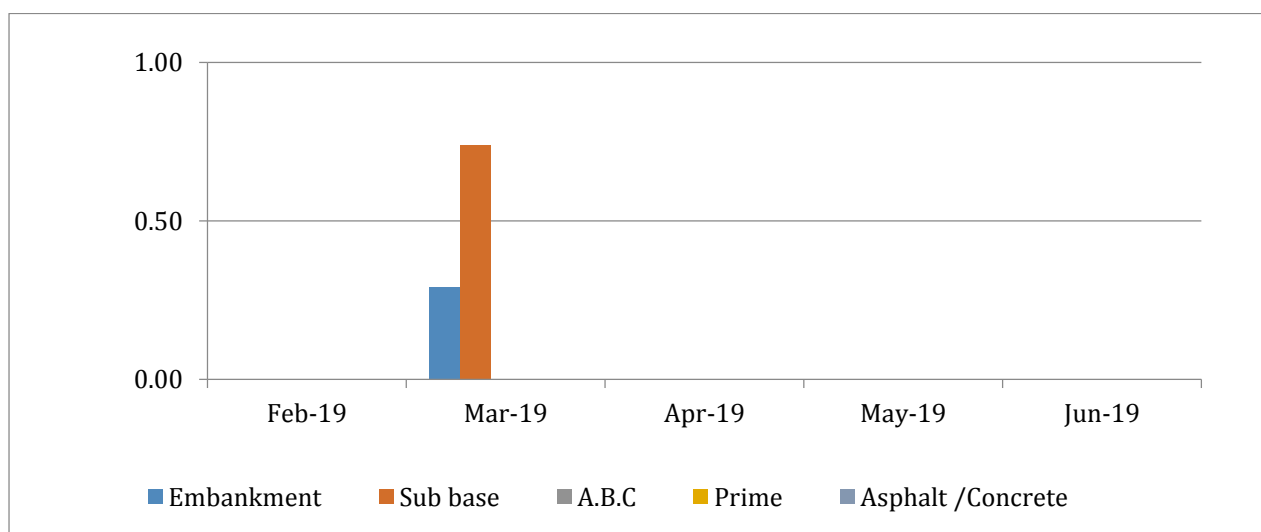
The Contract Package PU2 consists of 11 roads totaling 74.09 km and the accepted contract value is LKR 1,463,917,284.00. The Contractor has provided the Engineer's facilities as per the contract with the Construction Engineers Office situated in Chilaw. The Contractor's offices, yard and accommodation has been completed. The Contractor's main laboratory is operational. Physical S-Curve and Pre-Construction and Construction Activities are shown below:



Description	Total Length (km)	Horizontal Alignment		Initial Cross section		Investigation			Pavement Design		Vertical Alignment		Design Cross Section	
		Submitted	Approved	Submitted	Approved	Test pit	Lab Testing	Report	Submitted	Approved	Submitted	Approved	Submitted	Approved
2	6.73	6.73	6.73	6.73	6.73	6.73	6.73	6.73	6.73	6.73	6.73	6.73	6.73	6.73
3	9.83	9.83	9.83	9.83	9.83	9.83	9.83	9.83	9.83	9.83	9.83	9.83	9.83	9.83
13	6.15	6.15	6.15	6.15	6.15	6.15	6.15	6.15	6.15	6.15	6.15	6.15	6.15	6.15
26	10.95	10.95	10.95	10.95	10.95	10.95	10.95	10.95	10.92	10.92	10.92	10.92	10.92	10.92
28	1.97	1.97	1.97	1.97	1.97	1.97	1.97	1.97	1.97	1.97	1.97	1.97	1.97	1.97
30	3.10	3.10	3.10	3.10	3.10	3.10	3.10	3.10	3.10	3.10	3.10	3.10	3.10	3.10
45	1.70	1.70	1.70	1.70	1.70	1.70	1.70	1.70	1.70	1.70	1.70	1.70	1.70	1.70

Note: Road ID No. 4, 9, 27, 29 Completed.

PU2 Description							Total (Up to Jun-19)
	Up to Jan-19	Feb-19	Mar-19	Apr-19	May-19	Jun-19	
Embankment	39.63	0.00	0.29	0.00	0.00	0.00	39.92
Sub base	18.12	0.00	0.74	0.00	0.00	0.00	18.86
A.B.C	32.65	0.00	0.00	0.00	0.00	0.00	32.65
Prime	32.50	0.00	0.00	0.00	0.00	0.00	32.50
Asphalt/concrete (Total Length 74.09 km)	30.70	0.00	0.00	0.00	0.00	0.00	30.70



Road No.	Total Culvert	Culvert Condition Report		New Culvert			Reconstruction Culvert			Redecking Culvert			Widening Culvert						Cleaning & Repairing of Culvert		
	Nos.	Submitted	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	LHS			RHS			Total Nos.	Completed	In progress
													Total Nos.	Completed	In progress	Total Nos.	Completed	In progress			
2	26	26		5			7						3			2			11		
3	43	43		1	1		14	12		1	1		13	3	1	10	2	1	14	11	
4	50	50					10	5	1	1			4	1	1	3	1	1	35		
9	7	7		1	1		4	3	1										2		
13	12	12		4		1	2						4	1	2	4	1	2	2		
26	29	29		2	2		7	6								1	1		19	19	
27	46	46					5	2		1		1	7	5	2	8	6	2	31	31	
28	8	8					3						1			1			3		
29	21	21		1			12	5					3			2			4		
30	6	6					3						2			1			1		
45	8	8					1						4			4			3		
Page Total	256	256	0	14	4	1	68	33	2	3	1	1	41	10	6	36	11	6	125	61	0

Serial No	DS.Division	Road ID	Road Name	Road Category	Length (Km)	Progress Up to End of Progress Up to End of June 2019												
						1 km	2 km	3 km	4 km	5 km	6 km	7 km	8 km	9 km	10 km	11 km	12 km	13 km
1	Pallama	3	Adammana Junction to Madawakkulama Road	PS	9.78													
2	Mahakubukkadawala	4	Mahakubukkadawala Kottukachchiya Road	PRDD	12.34													
3	Puttalam	9	Madyama Attavilluwa to Kiula Road	PS	3.67													
4	Arachikattuwa	26	Arachikattuwa to Pallama via Adippala	PRDD	10.91													
5		27	Nalladurankattuwa to Wendakaduwa via Aththanaganyaya	PRDD	12.52													
6	Chilaw	29	Thiththakade to Manuwangama via Thissogama Co-operative Shop	PRDD	5.26													

Note: Road ID No. 13, 2, 45, 28, 30 Earth work Construction Still Has Not Started.

■ Earthwork in progress
■ ABC Completed
■ Asphalt Completed

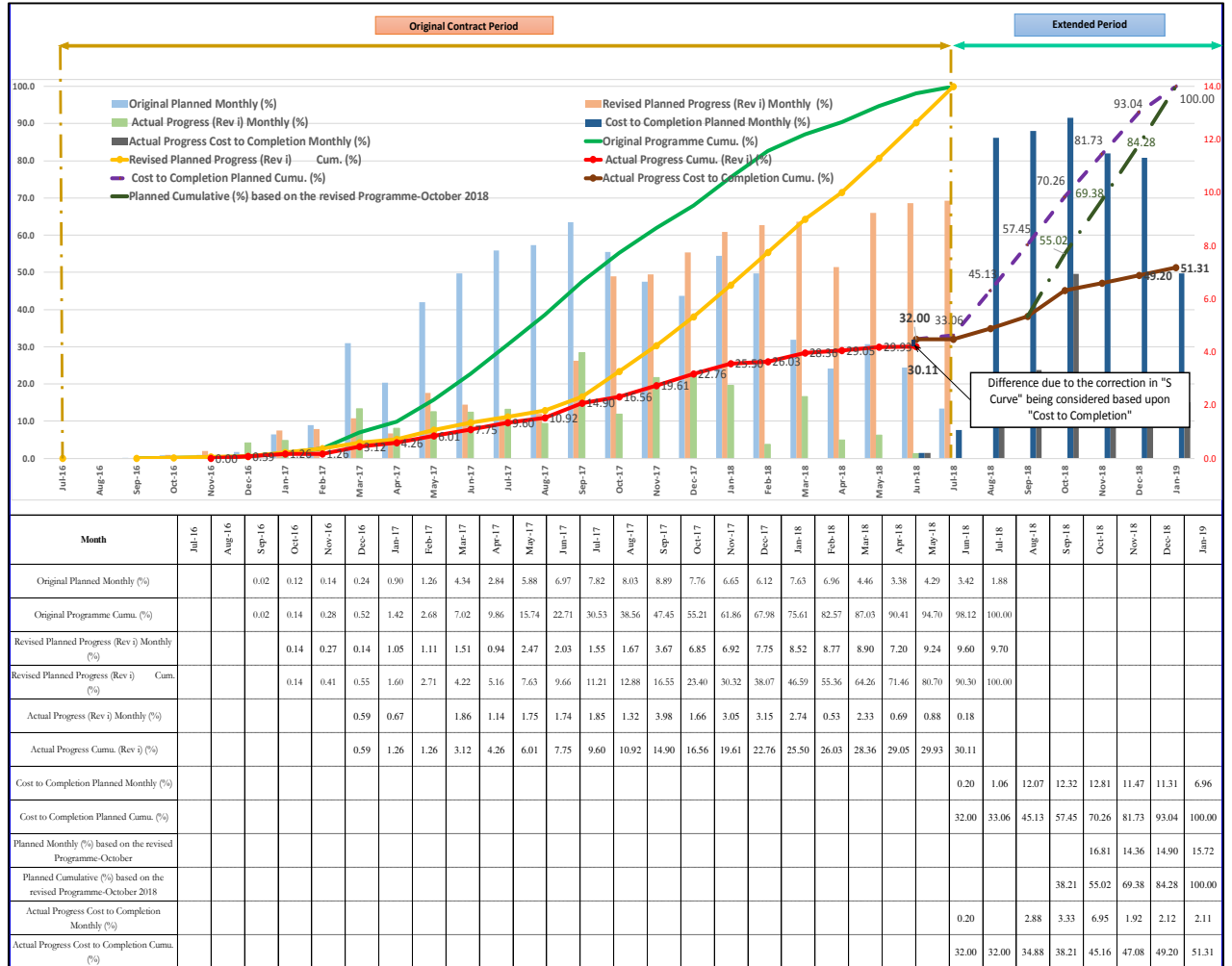
❖ Contract PU2 - Physical Progress

Progress	Cumulative Status as at 31 st May 2019		Cumulative Status as at 30 th Jun 2019	
	Target (%)	Target (%)	Target (%)	Actual (%)
Physical	77.74%	40.57%	83.63%	40.58%

The Engineer has recommended to the Employer to exercise its right to issue the Sub Clause 15.2 - Termination notice through letter ref. no. iROAD/PIC-02/RDA/PU2/01/4203 dated 13 March 2019. Action has to be taken by the Employer on this.

2.1.1.8 Contract PU3 – Contractor V. V. Karunaratne & Company

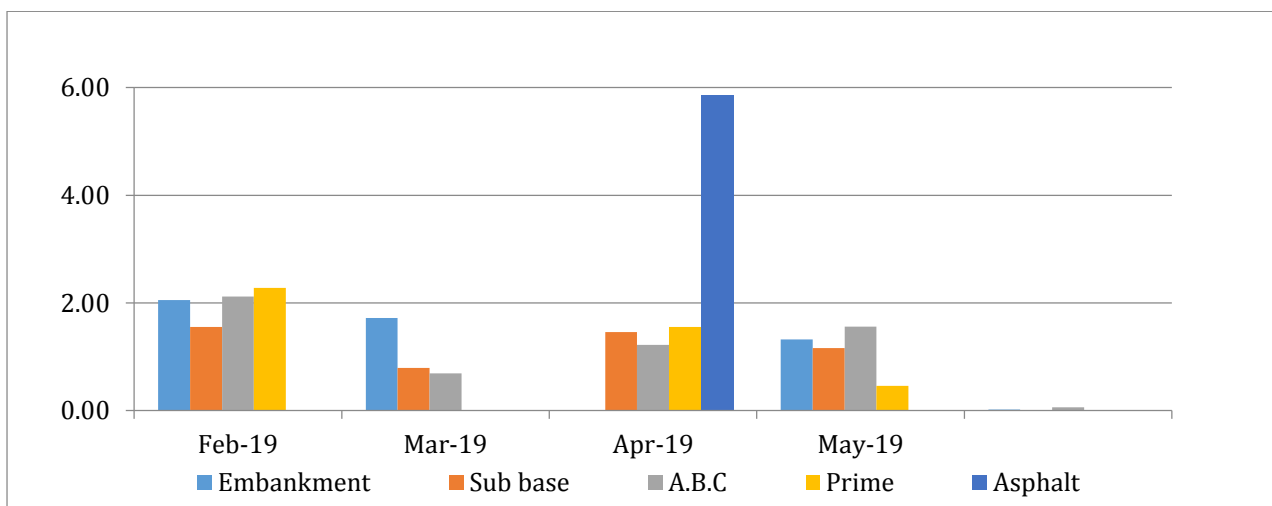
The Contract Package PU3 consists of 15 roads totaling 48.37 km and the accepted contract value is LKR 1,210,355,904. The Consultant's Resident Engineer's Offices are established in Marawila and the Construction Engineer's Office is established. The Contractor's main offices, yard and accommodation completed. The Contractor's main laboratory has been completed with the equipment supplied. Contractor's staff mobilization has improved. Physical S- Curve and Pre-Construction Activities are shown below:



Description	Total Length (km)	Horizontal Alignment		Initial Cross section		Investigation			Pavement Design		Vertical Alignment		Design Cross Section	
Road No.		Submitted	Approved	Submitted	Approved	Test pit	Lab Testing	Report	Submitted	Approved	Submitted	Approved	Submitted	Approved
16	2.64	2.64	2.64	2.64	2.64	2.64	2.64	2.64	2.64	2.64	2.64	2.64	2.64	2.50
17	3.86	3.86	3.06	3.86	3.86	3.86	3.86	3.86	3.86	3.86	3.86	3.86	3.86	3.75
21	6.79	6.78	6.78	6.78	6.78	6.79	6.79	6.79	6.79	6.78	6.78	6.78	6.78	6.53
22	5.09	5.09	5.09	5.09	5.09	5.09	5.09	5.09	5.09	5.09	5.09	5.09	5.09	4.86
24	1.49	1.49	1.49	1.49	1.49	1.49	1.49	1.49	1.49	1.49	1.49	1.49	1.48	1.48
31	1.69	1.69	1.69	1.69	1.69	1.69	1.69	1.69	1.69	1.69	1.69	1.69	1.69	1.69
32	2.42	2.42	2.42	2.42	2.42	2.42	2.42	2.42	2.42	2.42	2.42	2.42	2.38	2.38
33	2.89	2.89	2.89	2.89	2.89	2.89	2.89	2.89	2.89	2.89	2.89	2.89	2.89	2.89
34	4.27	4.27	4.27	4.27	4.27	4.27	4.27	4.27	4.27	4.27	4.27	4.27	4.27	4.22
36	4.29	4.29	4.29	4.29	4.29	4.29	4.29	4.29	2.90	2.90	4.29	4.29	2.90	1.90
38	2.33	2.33	2.33	2.33	2.33	2.33	2.33	2.33	2.33	2.33	2.33	2.33	2.33	2.33
39	1.85	1.85	1.85	1.85	1.85	1.85	1.85	1.85	1.85	1.85	1.85	1.85	1.84	1.84
40	2.91	2.91	2.91	2.91	2.91	2.91	2.91	2.91	2.91	2.91	2.91	2.91	2.91	2.91
41	3.40	3.40	3.40	3.40	3.40	3.40	3.40	3.40	3.40	3.40	3.40	3.40	2.28	2.27
42	2.45	2.45	2.45	2.45	2.45	2.45	2.45	2.45	2.45	2.45	2.45	2.45	2.43	2.43

Note: Road ID No. 31, 33, 38, 40 Completed.

PU3 Description							Total (Up to Jun-19)
	Up to Jan-19	Feb-19	Mar-19	Apr-19	May-19	Jun-19	
Embankment	35.62	2.05	1.72	0.00	1.32	0.02	40.73
Sub base	35.39	1.55	0.79	1.46	1.16	0.00	40.35
A.B.C	34.21	2.12	0.69	1.22	1.56	0.06	39.86
Prime	30.41	2.28	0.00	1.55	0.46	0.00	34.70
Asphalt (Total Length 48.37 km)	28.03	0.00	0.00	5.86	0.00	0.00	33.89



Road No.	Total Culvert	Culvert Condition Report		New Culvert			Reconstruction Culvert			Redecking Culvert			Widening Culvert						Cleaning & Repairing of Culvert			Concrete Line Drain			Retaining Wall			Cross Drain		
	Nos.	Submitted	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	LHS			RHS			Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress
													Total Nos.	Completed	In progress	Total Nos.	Completed	In progress												
16	10	10	-	-	-	-	1	1	-	-	-	-	5	5	-	1	1	-	6	4	-	-	-	-	-	-	-	-	-	-
17	16	16	-	2	-	-	3	1	1	-	-	-	2	1	1	1	-	-	9	1	-	-	-	-	-	-	-	-	-	-
21	20	20	-	2	-	-	8	5	1	-	-	-	1	1	-	1	1	-	10	-	-	-	-	-	-	-	-	1	-	-
22	8	8	-	-	-	-	3	-	2	-	-	-	1	-	-	2	1	-	3	-	-	-	-	-	-	-	-	-	-	-
24	5	5	-	-	-	-	1	-	-	-	-	-	2	-	-	2	-	-	2	-	-	-	-	-	-	-	-	-	-	-
31	2	2	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	-	2	2	-	-	-	-	-	-	-	-	-	-
32	8	8	-	-	-	-	4	3	1	-	-	-	-	-	-	1	-	-	4	2	-	-	-	-	-	-	-	-	-	-
33	7	7	-	1	-	1	1	-	-	-	-	-	-	-	-	-	-	-	5	1	1	-	-	-	-	-	-	-	-	-
34	7	7	-	-	-	-	3	-	-	1	-	-	2	-	-	2	-	-	1	-	-	-	-	-	-	-	-	1	-	-
36	8	8	-	2	-	-	3	-	-	-	-	-	-	-	-	-	-	-	3	-	-	-	-	-	-	-	-	-	-	-
38	5	5	-	-	-	-	-	-	-	-	-	-	2	-	-	3	-	2	2	-	-	-	-	-	-	-	-	-	-	-
39	7	7	-	-	-	-	1	-	1	-	-	-	1	1	-	1	1	-	4	-	-	-	-	-	-	-	-	-	-	-
40	13	13	-	1	-	-	2	2	-	-	-	-	3	1	-	4	3	-	7	-	-	-	-	-	-	-	-	-	-	-
41	6	6	-	1	-	-	-	-	-	-	-	-	1	-	-	1	-	-	4	-	-	-	-	-	-	-	-	-	-	-
42	3	3	-	-	-	-	-	-	-	-	-	-	1	-	-	2	1	1	1	-	-	-	-	-	-	-	-	-	-	-
Page Total	125	125	0	9	0	1	30	12	6	1	0	0	21	9	1	21	8	3	63	10	1	0	0	0	0	0	0	2	0	0

Serial No	DS.Division	Road ID	Road Name	Road Category	Length (Km)	Progress Up to End of June 2019						
						1 km	2 km	3 km	4 km	5 km	6 km	7 km
1	Madampe	31	Madampe Ihalagama Road	PS	1.69							
2		32	Suduwella Junction to Mukunuwatawana Road	PS	2.42							
3	Nattandiya	16	Megahawilla Junction to Yakkdessa Primary School Road	PRDD	2.65							
4		17	Bandirippuwa to Maningala via Sadanangama Katuwagoda Road	PRDD	3.86							
5	Mahawewa	21	Waduraba BO Tree to Walahapitiya Cemetery via Muttibadivila Post office Road	PRDD	6.79							
6		22	Yatakalana temple to Thabbowa kotabagaha Road	PRDD	5.09							
7		24	Gurugodalla to Kuliyaipitiya Nattandiya main Road	PS	1.49							
8	Wennappuwa	33	Srigampola Church to Zinnor Juncion via Aubowan Junction	PRDD	2.89							
9		34	Weralugaha road Balldi junction Bandaranayake janapadaya to Dummaladeniya Haldaduwana Road	PRDD	4.27							
10		36	Bandirippuwa-Kirimetiyana Jerad mawatha to end of Jaya Mawatha	PRDD	4.29							
11	Dankotuwa	38	Dekwela Road	PRDD	2.33							
12		39	Atiyawela Road (North)	PRDD	1.85							
13		40	Atiyawela Mohottimulla Road	PRDD	2.91							
14		41	Kirimatiyana Varalla Watta Road	PRDD	3.40							
15		42	Morukkuliya to Dankotuwa	PRDD	2.45							
Note: Earth Work Construction Started for all Roads.						<div><div></div>Earthwork in progress</div> <div><div></div>ABC Completed</div> <div><div></div>Asphalt Completed</div>						

❖ **Contract PU3 - Physical Progress**

Progress	Cumulative Status as at 31 st May 2019		Cumulative Status as at 30 th June 2019	
	Target (%)	Target (%)	Target (%)	Actual (%)
Physical	100.00%	59.73%	100.00%	60.00%

The Contractor has submitted their second EOT in January 2019. After the evaluation it had been forwarded to the Employer on 22 January 2019 and again it had been resubmitted on 28 January 2019 based on the Employer's comments. Thereafter the relevant supporting documents from the Metrological Department were forwarded to the Employer on 28 February 2019.

Engineer resubmitted the evaluation with their recommendation for the second EOT on 23 April 2019 based on the discussion held with the Employer and the Contractor at the RE office (Puttalam) on 3 April 2019, where certain agreement on the permissible events that can be considered for the second EOT has been established.

Engineer submitted the original rainfall records from the Metrological Department on 07 May 2019 based on the comments forwarded by the Employer on 26 April 2019. Subsequently the EOT claim had been forwarded to the Chairman-RDA through PD-Coordinating by the PD vide the letter Ref.No. RDA/Iroad/PD-NWP/CP/08-02 dated 15 May 2019.

Engineer resubmitted the second and third EOTs on 30 May 2019 and 01 June 2019 respectively based on the comments received from the Employer vide the letter Ref.No. RDA/Iroad/PD-NWP/CP/08-02 dated 29 May 2019. The second EOT was approved by the Employer on 17 June 2019.

Third EOT was resubmitted to the Employer on 18 June 2019 after Employer approving the second EOT on 17 June 2019 as per the Employer's letter Ref.No. RDA/Iroad/PD-NWP/CP/08-02 dated 06 June 2019.

The Contractor had a discussion with their bank, HNB in March and they had managed to get an approval for Rs 100 million and received about Rs 40 million from the bank in early April. The Contractor has started their asphalt work during the first week of April and they had managed to complete around 5.86 km of asphalt works before the April new year holidays. Currently there is a balance of 14.48 km of asphalt to be completed under this contract.

Presently, the Contractor has applied for the release of their retention money and the Engineer has approved it backed by a bank guarantee for the retention amount. Further to this, the Employer has requested further clarification/document from the bank for their assurance in providing the facilities to complete this project PU3 till completion. This is again being taken up with the bank.

Since, there is a delay in the release of the retention amount as detailed above, the Contractor has no funds with them and the progress has been affected.

❖ **Contract PU3 - Progress Photographs**



Before construction



Concreting of bridge deck at Road ID 21
3+725 km

2.1.1.9 Progress Summary of Culverts -North Western Province

District	Contract	New Culvert			Reconstruction Culvert			Redecking Culvert			Widening Culvert						Cleaning and Repairing of Culverts		
											LHS			RHS					
		Total Nos.	Completed	Total Nos.	Completed	In progress	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress	Total Nos.	Completed	In progress
Kurunegala	KU1	15	15	0	82	82	0	8	8	0	22	22	0	18	18	0	140	140	0
	KU2	22	22	0	121	121	0	1	1	0	48	48	0	39	39	0	250	250	0
	KU4	21	16	2	61	44	2	14	12	0	27	21	0	30	23	0	238	69	5
Puttalam	PU1	36	32	0	41	40	0	1	1	0	25	25	0	31	31	0	117	18	1
	PU2	14	4	1	68	33	2	3	1	1	41	10	6	36	11	6	125	61	0
	PU3	9	0	1	30	12	6	1	0	0	21	9	1	21	8	3	63	10	1
Total		117	89	4	403	332	10	28	23	1	184	135	7	175	130	9	933	548	7

2.1.1.10 Status of GRC Committee Formation in Kurunegala & Puttalam Districts - NWP

Contract No.	Contractor	No. of Roads	No. of Divisions	No. of GRC (Level 3 - DS) formed	No. of GRC (Level 2 - GN) formed
KU1	MAGA	16	7	7	17
KU2	KDAW-BMG JV	19	8	8	20
KU3	Ranken	12	6	6	15
KU4	VVK & Co.	12	8	8	12
KU5	Ranken	17	10	10	18
TOTAL		76	39	39	82

Contract No.	Contractor	No. of Roads	No. of Divisions	No. of GRC (Level 3 - DS) formed	No. of GRC (Level 2 - GN) formed
PU1	Ranken	11	6	6	11
PU2	VVK & Co.	11	7	7	11
PU3	VVK & Co.	15	5	5	15
TOTAL		37	18	18	37

2.2. Present Status of CRC Contracts - North Central Province

2.2.1 CRC's in Anuradhapura & Polonnaruwa Districts

2.2.1.1 Contract AP1 - Contractor Maga Engineering PLC

The Contract Package AP1 consists of 17 roads totaling 85.09 km and the accepted contract value is LKR 1,656,915,760.00. Please refer to the **Table 2.3** (Page 5) for further details.

❖ The Engineer has recommended the EOT-2 up to 30 April 2019 and forwarded the documents to the Employer on 08 June 2019.

❖ All the major road works were completed by April 2019 and preparation of SAC is in Progress.

❖ Contract AP1-Roads Handed Over to Client-RDA

Serial No.	Road No.	Road Name	Road Category	Length / km
1	1	Thalawa -Jayagaga 149 Hagurankethagama	PRDA	10.24
2	2	Eppawala Police Junction Jaya Ganga Rotawewa	PRDA	2.89
3	3	15 Kanuwa - Kiriamunukole -Mawathawewa	PS	2.72
4	10	Mahawelithenna Ralapana Janapadaya Road	PS	5.27
5	12	Pahalamaragahawewa Katupathwewa	PS	3.00
6	36	Yaya4 Junction Bogas Handiya Kiralapetiyawa Halambewewa Road	PRDA	11.21
7	37	Maningamuwa Junction Ihala Oyamaduwa Nawodagama Road	PRDA	6.00
			PS	7.19
8	38	4 Ela Junction Mankadawala Jaffna Road	PS	4.49
9	39	4 Ela Junction Parasangaswewa Hospital Road	PS	4.47
10	40	Salaiyawewa Mawatha Thannayakulama Road	PS	2.33
11	41	Sucharithagama Yahalegam Temple Road	PS	4.50
12	43	CEB Deport Pahala Keerikkulama -Keerikkulama Nochchaduwa Road	PS	3.67
13	45	Abyepura Housing Sheme to Tammennapura	PS	3.20
14	46	Nagasena Mawatha	PRDA	1.94
15	61	9 Ela Janahitha Junction 7 Ela Thuruwila	PRDA	4.76
16	63	Rathmale Nachchaduwa	PRDA	2.84 0.30
17	65	Nelubuwa Bidu	PS	4.37
Total			-	85.39

2.2.1.2 Contract AP2 - Contractor Maga Engineering PLC

The Contract Package AP2 consists of 12 roads totaling 76.85 km and the accepted contract value is LKR 1,824,178,003.20. Please refer to the **Table 2.3** (Page 5) for further details.

❖ All the major road works were completed by January 2019 and preparation of SAC is in Progress.

❖ Contract AP2-Roads Handed Over to Client-RDA

Serial No.	Road No.	Road Name	Road Category	Length / km
1	19	Elagamuwa Junction Canal Road Horapola New Mosque Road	PS	3.27
2	23	Dabawatana Road	PS	4.24
3	26	Balaluwewa Karawilagala Road	PS	3.83
4	27	Kudawatagala Dambuluhalmillewa Road	PRDA	4.48
		Kudawatagala Dambuluhalmillewa Road	PS	4.87
5	28	Ulpothagama Junction Morotthegama Road	PS	4.78
6	66	Thammennawawa Junction Kumbukwewa Siyabalawewa Hunupalagama Road	PRDA/PS	10.98
7	67	Kalawewa Aukana Road	RDA	4.53
8	68	6 Ela Walpaluwa to Kagama 2 Ela Road	PS	4.72
9	69	Mahailuppallama Farm Akkara 100 to Senapura Katiyawa Road	PS	4.94
10	47	Thambuththegama Rajanganaya Road	PRDA	16.86
11	48	Ayurvedic Hospital Nawagattegama Road	PRDA	3.38
12	49	Eriyagama Paindakulama Road	PRDA	5.97
Total			-	76.85

2.2.1.3 Contract AP3 - Contractor Maga Engineering PLC

The Contract Package AP3 consists of 12 roads totalling 82.66 km and the accepted contract value is LKR 1,492,772,165.28. Please refer to the **Table 2.3** (Page 5) for further details.

❖ The Engineer has recommended the EOT-2 up to 30 April 2019 and forwarded the documents to the Employer on 17 June 2019.

❖ All the major road works were completed by April 2019 and preparation of SAC is in Progress.

❖ **Contract AP3-Roads Handed Over to Client-RDA**

Serial No.	Road No.	Road Name	Road Category	Length / km
1	5	Matale Junction Samagipura Road	PS	1.72
			MC	0.92
2	6	Kurundankulama School via Kalaththewa Matale Junction Road	PS	2.83
3	7	Mahakanadarawa Left Bank Elakanda Road	PRDA	5.40
4	8	Kannattiya Ashokapura Road	PS	2.76
5	9	Welankulama Junction Alappamkulama Road	PRDA	7.77
6	70	Katukeliyawa Ihala Halmillewa Sivalakulama Road	PRDA	5.37
7	24	Palugaswewa Mahakekirawa Road	PRDA	9.12
8	25	Laksirigama Puwakpitiya Kudarambewewa Road	PS	6.62
9	71	Palugaswewa -Udakadawala	PS	1.70
10	29	Yakalla Palugollagama Upuldeniya Road	PS	12.12
11	30	Galkulama Sivalakulama Yakalla Road	PRDA	7.82
12	60	A-9 Road Labunoruwa Muriyakadawala Road	PRDA	18.51
Total			-	82.66

RD ID 29 – Macadam surface for 3 bunds (1.528 km) will be carried out during the DNP.

2.2.1.4 Contract AP4 - Contractor Maga Engineering PLC

The Contract Package AP4 consists of 19 roads totalling 90.20 km and the accepted contract value is LKR 1,635,133,956.00. Please refer to the **Table 2.3** for further details.

❖ All the road works were completed by December 2018 and SAC is submitted to the employer on 29 June 2019.

❖ Contract AP4-Roads Handed Over to Client-RDA

Serial No.	Road No.	Road Name	Road Category	Length/ km
1	50	40 Kolaniya Bus Halt Nawagammanaya Road	PS	2.85
2	51	Thelmola Junction Milankulama AB Gammanaya Puhulagewewa Balaya Junction Road	PS	3.45
3	52	Mailgashandiya Daluggala Jayanthi Road	PS	3.49
4	53	Ruwanpura School Road	PS	1.03
5	55	Kobbekaduwa Road	PS	1.91
6	56	4th Post Thonigala Bridge Road	PS	1.61
7	57	Kanugahawewa Herathhalmillewa Road	PS	8.51
8	59	Halmillawatiya Yakawewa Road	PS	3.55
9	33	Elapathwewa Ritigahawewa Road	PS	3.81
10	34	Kapugollewa Maradanmaduwa Wagollewa Road	PRDA	4.18
11	35	Nanumillewa Junction to Vilewewa Road	PS	2.73
12	16	A9 Karapikkada Kurukkandegama Kebithigollewa	PRDA	6.05
13	17	Puhudiula Junction to Galkadawala Road	PRDA	3.88
14	18	Kirigalawewa Unagaswewa Moragoda School Road	PRDA	4.93
15	31	Kahatagasdigiliya Kainattama Road	PRDA	12.81
16	32	Rathmalgahawewa Road Kurukkuragama Junction to Hettikattiya Road	PS	7.00
17	13	Mahakanadarawa Thabowa Pansala Junction to Welioya Junction	PRDA	4.21
18	14	Ihalagama Junction Muslim Diulwewa	PRDA	8.33
19	15	Pihibiyagollewa 9th Post Ambagaswewa Road	PRDA	5.87
Total			-	90.20

2.2.1.5 Contract PO1 - Contractor Maga Engineering PLC

The Contract Package PO1 consists of 18 roads totaling 50.76 km and the accepted contract value is LKR 1,158,532,987.42. Please refer to the **Table 2.4** (Page 6) for further details.

❖ All the major road works were completed by September 2018 and SAC is submitted to the employer on 23 April 2019.

❖ Contract PO1-Roads Handed Over to Client-RDA

Serial No.	Road No.	Road Name	Road Category	Length / km
1	1	Ihakuluwewa Village Road	PS	3.77
2	2	Ganageyaya Village Road	PS	3.63
3	3	Athtanakadawala Pokunugala	PS	3.25
4	4	Welankattuwa Village Road	PS	2.12
5	5	Sarubima- Segala Village Road	PS	2.62
6	6	Bakamuna -Damanayaya Village Road	PS	2.26
7	7	Sirikanduyaya Track 18 Village Road	PS	2.17
8	8	Orubendisiyambalawa Village Road	PS	1.75
9	9	Bisokotuwa-Konduruwawa	PS	1.87
10	10	Yaya 32 Temple - 33 Bisokotuwa Junction	PS	3.30
11	11	Athmalpitiya Junction -Laxauyana Siyabalagaswewa Junction	PRDA	4.70
12	12	Palugasdamana Senanayake Road	PS	1.21
13	13	Parakramasamuthra- Kalahagala Village Road	PRDA	5.24
14	14	Kadawalawewa Village Road Stage 01	PS	1.75
15	18	DI Channel- Vijawahapura Village Road	PRDA	4.27
16	20	Onagama Main Road- Galkiriya Cemetery	PS	3.14
17	21	Parakramasamudraya- Ambanganga Village Road	PS	2.06
18	22	Parakramasamudraya- Thkunu Ela Village Road	PS	1.65
Total			-	50.76

2.2.1.6 Contract PO2 - Contractor Maga Engineering PLC

The Contract Package PO2 consists of 25 roads totaling 73.36 km and the accepted contract value is LKR 1,483,692,044.52. Please refer to the **Table 2.4** (Page 6) for further details.

❖ All the major road works were completed by December 2018 and SAC is submitted to the employer on 24 June 2019.

❖ Contract PO2-Roads Handed Over to Client-RDA

Serial No.	Road No.	Road Name	Road Category	Length/ km
1	24	Hingurakgoda- 7th Mile's Post -Gallamuna Hingurukdamana	PRDA	4.12
2	25	Weerapura 317-Karathakade Junction	PRDA	3.98
3	27	BOP 317- Dalpalama	PRDA	1.29
4	28	BOP 316- Karuwalagas Junciton -Chandanapokuna	PRDA	3.04
5	29	Patunagama Junction -Damwelmangada- Buthayaya	PS	3.48
6	31	Kubukkanaruwa- 127 Bawdarthgama	PS	1.50
7	72	Deberella Sudupalama-Gallamuna	PS	4.60
8	33	Track 12 Bridge -Nagarapura-Weligampura	PS	7.06
9	34	Yuthaganawa -Irrigation Junction- Dinisuru Junction	PRDA	3.04
10	35	Kusumpokuna- Pimpura	PRDA	3.51
11	36	Pimpura Pansalgodall Main Road- Sansugama 21 Division	PRDA	1.88
12	37	Meegaswewa- Wadigawewa	PS	3.08
13	40	Medirigiriya Town Internal Road	PS	1.88
14	41	Jayathugama Junction Meegollawa School	PS	3.46
15	42	Medirigiriya Water Tank- Mr. Vijayadasa's House	PS	2.23
16	44	Medirigiriya Kalagedi Palama- Bisobandaragama School	PS	2.55
17	73	Diulankadawela Aliwankuwa Junction	PS	3.13
18	45	Minneriya Central Collage Road	PS	3.05
19	46	Hathamuna Bridge-Nugagahadamana Road	PS	3.71
20	47	Dora 2 Junction-Paluwewa	PS	1.81
21	48	Girithale Middle Road	PS	1.49
22	49	Hingurakgoda Airport-Vidyaloaka School Road	PRDA	1.41
23	51	Batukotuwa Middle Road	PS	1.13
24	52	Minneriya Samagipura	PS	2.37
25	55	Hinguraka 4th Miles Post -Chandanapokuna Aliwanguwa	PS	4.56
Total			-	73.36

2.2.1.7 Contract PO3 - Contractor TISSA-AMSK JV

The Contract Package PO3 consists of 11 roads totaling 49.55 km and the accepted contract value is LKR 1,249,343,460.00. Please refer to the **Table 2.4** (Page 6) for further details.

❖ All the major road works were completed by October 2018 and SAC is submitted to the employer on 15 May 2019.

❖ Contract PO3-Roads Handed Over to Client-RDA

Serial No.	Road No.	Road Name	Road Category	Length/ km
1	57	Welikanda-Singhapura Road	PS	12.03
2	58	Athugala -Katuwanwila	PS	6.80
3	59	Ginidamana-Nelumwewa	PS	2.10
4	61	Mahadamanawewa Village Road	PS	3.02
5	62	Dimbulgala Junction-Bogaswewa Village Road	PS	6.52
6	64	Manmpitiya Hospital Road	PS	1.24
7	65	2nd Mile's Post-Seelapura	PS	1.47
8	66	Siripura Nuwaragala Village Road	PS	9.62
9	67	Aluthoya Junction - Village Road	PS	2.03
10	68	Dimbulagala Junction-Soruwila Village	PS	1.75
11	71	Bandanagala-Bandanagala Village	PRDA	2.97
Total			-	49.55

3.0 Compliance with Social Safeguard Requirements in the Loan Documents

3.1 Compliance with respect to provisions in the Loan agreement

Relevant to the Loan Agreement Tranche. 4

The requirements stipulated in the loan conditions of Projects 4 and 5 have been and are being complied with as detailed in the below table.

Table.3.1 Compliance with Loan Conditions of project Social Safeguard activities

Item/Section/Schedule	Description	Status of Compliance
Schedule 4-Consulting services	The borrower shall recruit the individual consultants for activities outlined in the Procurement Plan in accordance with procedures acceptable to ADB for recruiting individual consultants	It has been completed following the recruitment as described in the Procurement Plan
Schedule 5 - Safeguards	The borrower shall ensure, or cause RDA to ensure, that the project does not have any Indigenous Peoples Safeguards and Involuntary Resettlement Safeguards impacts both within the meaning of SPS. In the event that the Project does have any such impact, the Borrower shall take all steps required to ensure that the project complies with the RF and IPPF, applicable laws and regulations of the borrower and the SPS	No indigenous people or communities identified during preliminary social studies/public consultations in the NCP and NWP area. All rural roads have been selected for Projects 4 and 5 are between 2.5 m to 5.5 m width of ROW and there is no need of land acquisition or resettlement due to the project.
	The Borrower shall ensure, or cause RDA to ensure, that any voluntary donation of land by beneficiaries for any provincial or rural road is implemented in accordance with the procedures laid out in Appendix 3 of the RF after free, prior and informed consultation and consent of the affected persons, provided that there shall be no voluntary land donation for national roads.	When it is important and insisted by the beneficiaries to use their piece of land for construction of a road section the procedure will be followed accordance with Appendix 3 of the RF in respect of voluntary land donation. During the period up to June 2019, voluntary land donation activities have been taken place in NCP and NWP complying with RF directions.
Schedule 5- Labour Standards	The Borrower shall ensure that (a) all contractors under the Project comply with all applicable labour laws and regulations, do not allow employment of child labour for Project activities, encourage the employment of the poor, particularly women, and provide appropriate facilities for women and children in work sites; (b) people directly affected by the Project are given priority to be employed by the Project; (c) contractor do not differentiate wages between men and women for work of equal value; and	It is being observed during the period of January-June 2019 by the Consultants staff (PIC2) especially by SGRS, ES, RE and ARE, that the status of the contractors' compliance with existing labour laws during implementation of construction were satisfactory. A very few contractors were advised by Consultants when they were failed to comply with labour laws. Also the contractors are advised not to use child labour and encourage them to recruit

	(d) specific clauses ensuring these shall be included in the contracts	unemployed/poor people and project influenced people as workers. During the reporting period no any contractor has recruited children for their works. It is continuously advised that the contractors recruit women as much as possible as construction site staff while paying equal payment for men and women for same value of work.
Schedule 5 – Gender Considerations	The Borrower shall ensure that (a) the GAP is implemented in accordance with its terms; (b) the bidding documents and contracts include relevant provisions for contractors to comply with the measures set forth in the GAP; (c) adequate resources are allocated for implementation of the GAP; and (d) progress of implementation of the GAP, including progress towards achieving key gender out come and output targets, are regularly monitored and reported to ADB	Contractors were advised to do public consultations during GRC formation meetings, refreshing transect walks, level 2 and 3 GRC meetings, finalizing road designs, health and road safety awareness programs while ensuring the participation of women. Ans such activities were monitored by PIC2 and PIU. Social and Environment safeguard officers of the contractors were advised to ensure the above activities and SGRS monitored relevant activities during refreshing transect walks and GRC formation and now during monthly progress review meetings. Monthly progress review meetings are used by the SGRS to meet all contractors and monitor status of contractor's compliance of the GAP. Fourteen public health and HIV/AIDS awareness programs have been conducted up to June 2019 complying with GAP. Prioritized more village level women participation in these programes.

3.2 Compliance to the Social Requirements under the Project Facility Administration Manual (FAM)

The requirements have been described in the project Facility Administration Manual of Integrated Road Investment Program have been and are being fulfilled in Project 4 and 5 of Tranche 4. Details of each item which is relevant as reported by July-December 2018 report is summarized in the matrix below considering activities carried out during period of January-June 2019.

Table 3.2. - Compliance with FAM (Social Safeguards)

Item/ Section/ Schedule	Description	Status of Compliance
Pre-construction	59. The RDA supported by its ESDD and context sensitive design consultants (CSD) will be responsible for conducting a transect walk for each rural road, during which the community will be consulted on how to (i) minimize construction impact, (ii) identify and minimize social and environment impact, and (iii) integrate road safety design into road. The community will also be briefed about the Grievance Redress Mechanism (GRM). The results from the transect walk (report and map) will be submitted to the civil works contractor who will reform the ground realities and integrate feasible features into the Level 1 final design. The PIC will review final design.	<p>Transect walks have been conducted for all candidate roads during pre-construction stage. Community participation has been a key feature of these transect walks.</p> <p>The outcome of the transect walks and video footage have been provided to the respective contractors to consider feasible features in the designs.</p> <p>Refreshing transect walks carried out with the guidance with PIUs and PIC2 for Project 4 and 5 during the period of establishing GRC committees. All transect walks have been completed in two projects during 2018.</p>
Construction and maintenance.	60. During construction phase, the PICs will be responsible for conducting semi-annual onsite social safeguard monitoring. During the maintenance phase, the PIC will be responsible for conducting annual onsite social safeguard monitoring. The PICs should pay close attention to ensuring that all grievances are addressed in a timely manner. Reporting templates can be found in the Resettlement Framework.	<p>PIC2 is paying close attention to address all the public grievances and monitor onsite social safeguards. PIC-02 recruited four District Social & Environmental Assistants (DSESA) during January-March of 2018 for four district Resident Engineers Offices to assist SGRS and ES at the TL the office in monitoring safeguard activities and all safeguard activities in each district have been monitoring by them while participation in field programs. DSESAs were provided informal orientation programs to familiarise relevant activities just after recruitments by ES and SGRS. ES and SGRS coordinate them directly and also with ESOs and PMs of the 15 contract packages on a monthly basis during MPRMs.</p> <p>The Master Complaint Register is being maintained by the all contractors successfully in PIC2. DSESAs were asked to monitor such activities. SGRS individually meet them and assist to keep documents and to do field work to</p>

Item/ Section/ Schedule	Description	Status of Compliance
		<p>ensure proper implementation of social safeguard activities. All relevant requirements reviewed during monthly progress review meetings during the period up to end of June 2019. However similar safeguard monitoring activities of SGRS and DSESAs in NCP was gradually reduced since the NCP construction of roads in 7 packages were completed as at January –March 2019. Roads handed over to the Employer and PBM period started. Consultants’ staff was also reduced in District Offices during June 2019. ESOs of the contractor and DSESAs were also demobilized due to completion of road works. However ESOs of KU-03 and KU-05 left the project after termination those contract packages.</p>
<p>Preparation of Subsequent Tranches or Unanticipated Impact.</p>	<p>61. A Resettlement Framework has been prepared to guide the preparation of all tranches. For preparation of subsequent tranche, the PICs will carry out due diligence on the ongoing tranches.).The RDA will be responsible for initial road selection based on growth potential of Grama Niladari (GN) Divisions as rural hub. The Environment and Social Development Division (ESDD) will then conduct land acquisition due diligence and social impact assessment. All the affected households will need to be covered and 20% census of households in the project impact area will be surveyed. If land is required for the rural roads component, the voluntary land donation system will be used. For land donation, specific procedural requirements involving comprehensive consultations with the communities have been prepared in Appendix 3 of the resettlement framework (RF), and the Government will ensure that land donation is undertaken without coercion and documented in a transparent manner.</p>	<p>Already completed by ESDD during pre-project implementation stage. Since no land acquisition in this program the completed due diligence reports have fulfilled other social safeguard requirements. Land Donation activities are being carried out as described in the RF and in a transparent manner with proper public consultations by the PIUs and PIC2. More information on land donation activities during the period of January-June 2019 will be in the section 07 in this report. During the reporting period there were only 14 consent letters received for donation of land strips to construct roads in NCP and NWP. Altogether 875 consent letters have been received by PIUs in project 4 and 5 as at June 2019.</p>
	<p>Eminent domain will not occur if negotiation regarding land donation fails. Households will not donate more than 5% of land holding and no physical displacement will take place. The PIC will visit roads with land donation on a</p>	<p>Land acquisition is not applicable to iRoad Program.</p> <p>PIC2 and PIUs always ensured that there are no any ‘Eminent Domain’</p>

Item/ Section/ Schedule	Description	Status of Compliance
	<p>monthly basis until land donation process is finalized. An independent monitor will be engaged to verify and certify the process has been done as per the resettlement framework. For national roads requiring land, acquisition will take place in accordance with requirements of the Government and the ADB's SPS, and land and assets will be compensated at replacement cost for both titleholders and non-titleholders. A Resettlement Plan will be prepared for projects with impact. For all roads, civil works cannot commence until the right of way is free of encumbrance, meaning that the affected household must be fully compensated and/or assisted prior to dismantling or relocation. The PIU will be supported by the RDA's ESDD and a social safeguard team under the PICs.</p>	<p>practices and receiving more than 5% land as donations.</p> <p>Other policies related to voluntary land donation being complied with in projects 4 and 5. As stated below there are 875 cases of voluntary land donations through signing of consent letters as at end of June 2019. 14 consent letters received during reporting period. Details of land donation are discussed separately in the section:07 of this report.(See the Table 7.1)</p>
VII Safeguards – C. Indigenous Peoples	<p>62. For the investment program, an Indigenous Peoples Planning Framework (IPPF) has been prepared to guide the screening and preparation of safeguard planning for future tranches.</p>	<p>There are no roads that are passing close to or through settlements of indigenous people. Even during refreshing transect walk surveys no such locations were observed within project area of project 4 and 5.</p>
	<p>61. For Tranche 1 and 2, no indigenous people were identified during due diligence and are categorized as "C" per ADB SPS. Tranches 3 and 4 will continue to finance the sub projects identified in the two earlier tranches; therefore, both tranches will follow the categorization of "C". In case any adverse impacts are identified during implementation, the RDA will ensure that the Indigenous peoples plan (IPP) is prepared in accordance with the IPPF and the IPP is reviewed and approved by ADB before commencement of the relevant section of the civil works contract.</p>	<p>There are no roads that are passing close to or through settlements of indigenous people. During refreshing transect walk surveys no such locations were observed within project area of project 4 and 5.</p>

Item/ Section/ Schedule	Description	Status of Compliance
D. Grievance Redress Mechanism	<p>64. The Grievance Redress Mechanism (GRM) will be established consistent with the requirements of the EARF. Grievances from the affected people on social and environmental issues during project implementation will be addressed mainly through the existing local administration system.</p> <p>Depending on the nature and significance of the grievances or complaints, grievances will be addressed at three levels: (i) at the grassroots level where complaints will be directly received and addressed by the contractor, the PIC or PIU representative onsite; (ii) grievances that are simple but still cannot be addressed at the grassroots level will be addressed at the Grama Niladhari division level; and (iii) more complex grievances that cannot be addressed at the Grama Niladhari division level will be addressed at the Divisional Secretariat level. There will be a grievance redress committee at both the Grama Niladhari division and Divisional Secretariat levels.</p>	<p>Being complied with in projects 4 and 5. Since all GRCs have been established from 2016 to 2018 period no any GRCs have established during reporting period. It has been completed establishment of 236 GN level GRCs and 84 DS level GRCs as at end of June 2019. Separate section is discussing (5.5) the progress of GRM in this report. No any level 2 or 3 GRC meetings were held in NCP area since road construction was completed and all the roads were handed over to the Employer. However public complaints receiving even during PBM period were being entertained by the contractor and the Employer during reporting period.</p>

3.3 Compliance with requirements in the Resettlement Framework (RF)

These rural roads have been selected during 2014 using information provided by government officers, civil societies and village leaders through District Secretaries and Divisional Secretaries. Initial investigations to verify the availability of required ROW have been carried out by RDA. Other requirements stipulated in the RF have been and are being complied as summarized under table 3.3.

Table 3.3. Compliance with the Resettlement Framework

Item/ Section/ Schedule	Description	Status of Compliance
A. Background	<p>4. Rural Roads. For the rural roads there will be three contract packages per district. The contractor will be responsible for construction of the road over 2 years and performance based maintenance for another 3 years. For the rural roads the improvements will be undertaken completely within the existing right of way which is between 2.5m to 5.5m. Rehabilitation works will include improving pavements/road surface, construction of side drains and</p>	<p>Complied with in selection of roads. Three contract packages are in each district of Puttalam and Polonnaruwa and four packages in Anuradhapura and five packages in Kurunegala districts. No road having an average ROW of less than 2.5 m was selected. Locations where improvements to road structures and safety needs have been highlighted in transect walk records.</p>

Item/ Section/ Schedule	Description	Status of Compliance
	embankments, and improvement of culverts, causeways and bridges.	
B. Screening Criteria of Subsequent Projects	<p>44. The PIU will be responsible for clearly documenting the answers to these questions in a table format. In addition, the PIU will submit to ADB the Involuntary Resettlement (IR) Impact Categorization Checklist in Appendix 1 for each subsequent tranche. Based on the screening, the tranche will be assigned to one of the following categories depending on the significance of probable involuntary resettlement impacts:</p> <p>(i) Category A – A proposed tranche is classified as Category A if it is likely to have significant involuntary resettlement impacts. A resettlement plan, including an assessment of social impacts, is required.</p> <p>(ii) Category B – A proposed tranche is classified as category B if it includes involuntary resettlement impacts that are not deemed significant. A resettlement plan, including assessment of social impacts, is required.</p> <p>(iii) Category C – A proposed tranche is classified as Category C if it has no involuntary resettlement impacts. No further action is required.</p>	<p>Complied during SAPE works stage of iRoad program.</p> <p>Screening of all roads has been completed.</p> <p>No land acquisition or resettlement requirements. Tranche 2&3 has categorized as “B” in updated FAM of March 2018.</p>
C. Land Acquisition Due Diligence Reports	<p>48. If no acquisition and resettlement impacts were identified during screening, the government will be required to prepare a due diligence report providing support and evidence that there is no impact on land and structures. The due diligence report should confirm whether there are outstanding grievances or issues from previous acquisition of land. The information in the report should be supplemented with findings and analysis from the socioeconomic survey comprising of 20% of households in the project affected area.</p>	<p>Compiled during SAPE works. Involuntary Resettlement Due Diligence and Socioeconomic Assessment report has been prepared for roads in NCP and NWP. These reports had been disclosed prior to commencement of civil works.</p>

4.0 Compliance with Gender Action Plan (GAP)

4.1 As emphasized in the schedule 5 of the iRoad Loan Agreement ‘the borrower shall ensure that (a) the GAP is implemented in accordance with its terms; (b) the bidding documents and contracts include relevant provisions for contractors to comply with the measures set forth in the GAP; (c) adequate resources are allocated for implementation of the GAP and (d) progress of implementation of the GAP, including progress towards achieving key gender out come and out puts targets , are regularly monitored and reported to ADB.

4.2 As mentioned in previous semi-annual reports, Implementation status of GAP requirements have been monitored during this reporting period (January-June 2019) based on the key elements of the GAP. As the GAP emphasizes that the entire iRoad program is classified as an “Effective Gender Mainstreaming” or EGM. Gender mainstreaming activities include (i) ensuring that 40% women are consulted during project preparation; (ii) integrating features to increase safety and meet the safety needs and meet the needs of the elderly, women, children and differently abled in to the final design(EWCD); (iii) ensuring that at least 30% of local employees hired for road maintenance are women, and that they receive equal wages for equal work done for both skilled and unskilled labour in project works; (iv) provide road maintenance training to all women who are employed; (v) ensure women participation in road safety, HIV/AIDS and human trafficking awareness campaigns targeting local communities; (vi) appoint a social and gender focal point at each of the four project implementation units, trained by a specialist in PIC; (vii) build the capacity of RDA and the five provincial road agencies on gender inclusive designs and gender mainstreaming in project preparation, consultation, road construction and maintenance.

Table: 4.0 Complying status with the GAP,

	Requirement	Current Status
1	Incorporating EWCD requirements in to the final designs	<p>Being complied within projects 4 and 5. During the beginning of the project, awareness was created among road users and government officials at GRC formation meetings emphasizing the importance of EWCD friendly designs. Then the participants proposed their requirements at meetings as well as during refreshing transect walks. Difficulties due to narrow road sections, especially for women, elderly people and children who going schools, hospitals and religious places, the road sections with water stagnation etc. were found as common issues in two provinces. Contractor, Consultant and Employer included these public proposals in the final road designs as technically and financially appropriate.</p> <p>During the year 2018 and up to June 2019, required road markings and placing signage are completed in 160 roads in 12 packages in two projects which are fulfilling social safeguard requirmrnts of road users specially the social category of EWCD. During the reporting period it was 53 new roads. Locations at schools, hospitals, dispensaries, road bends etc. which are sensitive to women, children, adults and disabled people were specially considered for road marking and placing signage. It was reported last semi-annual report, as an example some actions have taken to change designs for the safety of children and parents at schools. The Walawwagama School in Kalawana-Aukana road at Rd ID 67 of AP-02 a U drain provided to control inundation in to the school during rainy days for the benefit of</p>

		children. And another two examples are found from RdId 102 of KU-02 assisting elderly women providing house access were not in original design. Relevant case studies were also included in the previous report. However the majority of design changes for the benefit of beneficiaries including EWCD category have been taken place in KU-02 as at end of June 2019. It was carried out in 63 locations in 16 roads in KU-02. More details on these benefits are being now collected.
2	40% women participation during project preparation	This has been complied with in projects 4 and 5 during SAPE works stage. Even during the refreshing transect walks and GRC establishing meetings the women participation was encouraged and considered as important.
3.	30% local employees for project activities –Road Maintenance	This requirement is for the maintenance period (PBM period) which was commenced from December 2018 in NCP and KU-01. Current status is included in the table: 4.3.1. Female participation is 19.3%. However, the PIU, PIC and Contractors have brought women as professional staff, office staff and unskilled labour. However due to termination of KU-03 and KU-05, and completion of road construction of NCP and KU-01, the workforce of the projects 4 and 5 has been reduced during reporting period. The Summary of gender composition during reporting period January to June 2019 in each PIU, PIC and Contractor are summarized in table 4.1. It is stated as 9.29% of female participation of the whole project.
4	Women participation in HIV/AIDS awareness program	Conducting awareness programs on HIV/AIDS for iRoad program implementation staff of Contractors, the Employer and Consultant was successfully completed during 2018, (up to end of December 2018), 15 awareness programs for project staff of 15 packages of two projects were successfully completed. Women participation in these programs was 12.67%. As GAP recommended similar health and HIV/AIDS awareness programs for villagers who living close to roads were conducted and 14 programs up to June 2019 completed, the last program implemented on 24 th January 2019, in AP-03, during reporting period. PU-03 program is still to be implemented.
5.	Provide road maintenance training to all women who are employed;	Road maintenance period started during December 2018. PIU, PIC and Contractor senior staff were provided with training and orientation during 2018. The contractors are now providing informal training to workers who involve in PBM. Current status of labor utilization in PBM in some packages are in the table:4.3.1
6.	Build the capacity of RDA and the five provincial road agencies on gender inclusive designs and gender mainstreaming in project preparation, consultation, road construction and maintenance.	With the initiation of the ESDD-RDA and ADB-SLRM, four gender training programs for engineering/technical, social and environmental staff of Contractor, Employer and Consultant were implemented in two projects during the previous reporting period (2018). 17 th May, 22 nd May, 7 th June, 19 th June 2018 four programs were held in Kurunegala, Puttalam, Polonnaruwa and Anuradhapura. At training sessions, concept of 'Gender' was clarified in to practical usage and explained the constructive role of men and women in the family and the society. Equity and equality of men and women collaborative works and the important areas to be considered during planning and implementation of road development projects considering gender equality and equity.

4.3 As the table above (Table: 4.0) describes, SGRS assisted the contractors to comply with gender considerations even during reporting period from January to June 2019.



Fig: 4.3 During Transect walks and GRC formation prioritized women participants at the begging of the program

Table: 4.1: GENDER STATUS as at end of June 2019

Package	Office		Construction Sites	
	Male	Female	Male	Female
Project:04 NCP				
AP-01	Performance Based Maintenance Period(PBM) Please see the table 4.3.1 on gender status in PBM period			
AP-02				
AP-03				
AP-04				
PO-01				
PO-02				
PO-03				
Sub Total-NCP				
Project:05 NWP				
KU-01	Performance Based Maintenance Period(PBM)			
KU-02	30	06	147	04
KU-03	Contract terminated during February 2019			
KU-04	29	05	66	02
KU-05	Contract terminated during February 2019			
PU-01	118	00	82	04
PU-02	10	04	16	03
PU-03	36	05	70	15
Sub Total	223	20	381	28
Grand Total	223	20	381	28
Female %	07.3%			
iRoad Program, PIC2 and PIU staff			Project Total	

	Male	Female		
PIU-Project -04	24	07	Male	Female
PIU-Project- 05	30	05	898	92
PIC2 Project -04	105	17	Female 09.29%	
PIC 2 Project-05	120	11		
PIC2 TL Office	15	04		
	294+604=898	44+48=92		

4.4 The Table 4.1 reports the gender status of iRoad program staff of the Contractor, Employer and the Consultant during the reporting period. Since, it was the period of road handing over in NCP and KU-01 of NWP the workforce has been gradually reduced from January to June 2019. Also KU-03 and KU-05 contract packages have terminated during the period. Women participation percentage is now become up to 9.29%.

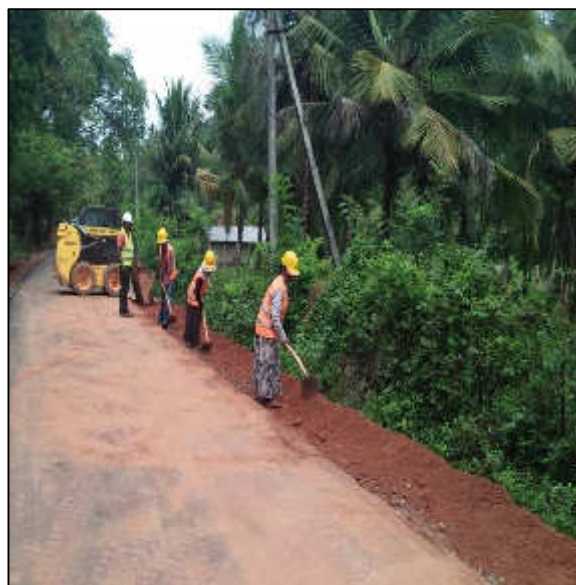


Fig.4.4 Women Participation in iRoad construction works



Fig: 4.5 Women Government Officers assist the iRoad program to resolve social issues



Fig.4.6 Women in villagers keen to know about the iRoad program and to provide their support

4.1 Elderly, Women, Children, Disabled (EWCD) friendly Designs

4.1.1 As a key social safeguard policy of Context Sensitive Designs (CSD) in the iRoad Program, it is prioritized EWCD considerations during planning, construction and operation stages of the program. As GAP describes the compliance of social safeguards requirements, during improving rural roads, it is important to integrate safety and elderly-women-children-disabled (EWCD) friendly features in to designs as outputs of the benefits of the integrated rural road investment program. In 53 new roads have completed with EWCD friendly traffic signages and road marking during the reporting period (Table: 4.2). Paved shoulders, pedestrian crossings and locations of signage are in designs would be good examples of the requirement. During the reporting period, planning and implementation of road marking and placing signage activities completed and also providing passing bays and parking bases as appropriate. Please see the table 4.2 below.

Fig: 4.1.1 Road Marking and Traffic signage ensure safety of road users specially EWCD

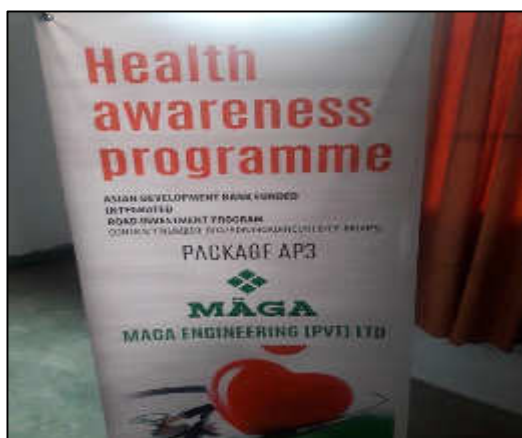


Fig: 4.1.2 Road Marking and Traffic signage ensure safety of road users specially EWCD

Table 4.2 : Roads which Road Marking and Traffic Signage completed

Package	Road IDs which <u>Road Marking</u> and <u>Traffic Signage</u> completed as at June 2019	As at end of 2018	As at end of June 2019
AP-01	1,2,3,10,12,36,37,38,39,41,43,45,46,61,63,65	07	16
AP-02	19,23,26,27,28,66,67,68,69,47,48,49	08	12
AP-03	5,6,7,8,9,70,24,25,71,29,30,60	06	12
AP-04	50,51,52,53,55,56,57,59,33,34,35,16,17,18,31,32,13,14,15	19	19
PO-01	1,2,3,4,5,6,7,8,9,10,11,12,13,14,18,20,21,22	18	18
PO-02	24,25,27,28,29,31,72,33,34,35,36,37,40,41,42,44,73,45,46,47,48,49,51,52,55	25	25
PO-03	57,58,59,61,62,64,65,66,67,68,71	11	11
KU-01	30,32,34,33,36,76,97,98,81,82,47,48,49,50,86,55	09	16
KU-02	56,57,58,59,1,2,4,100,101,5,102,41,43,45,46,46R,74,75,67,66	04	20
KU-03	71		01
KU-04	92,96		02
KU-05	-		
PU-01	8,44,11 (Road marking and Traffic Signage)and 43,5,14,15,1(Only Road marking)		08
PU-02	-		
PU-03	-		
Total		107	160

4.1.1 As the GAP recommends, HIV/AIDS, STD and public health awareness programs successfully completed during the program period from June 2016, up to end of June 2018. Fifteen HIV/AIDS and STD awareness programs implemented for the staff of Contractors, Employer and the Consultant and in 15 contract packages during 2018. Fourteen HIV/AIDS, STD and public health programs for community people close to roads which are developing have been implemented up to end of June 2019. A program in AP 3 implemented during the reporting period, on 24th January 2019. More details on the other programs included in the previous semi-annual report (July-December 2018).

**Fig: 4.1.3 Public Health Awareness Program of AP 3 held on 24th January 2019**

This program was organized by AP-03, MAGA contractor on 24.01.2019 at the Palugaswewa DS office. Among the objectives were, to provide awareness to local community on Kidney diseases, non-communicable diseases, sexually transmitted diseases, illegal drug use, Alcoholism and child abuse in society, minimizing the risk of transfer of the HIV virus between contractors and local communities, and developing personal motivation, knowledge and skills to choose healthy lifestyle and undertaking actions for improving own health etc.

Fig.4.1.4: Public Health Awareness Program of AP 3 held on 24th January 2019



4.2 Road Safety Awareness Programs

4.2.1 Previous semi-annual report included more details on the safety program. This program was mainly implemented for addressing three social categories who are important to road safety, such as school children, three wheel drivers and motor cyclists. Reason is majority of rural roads users are among them. And also, it is expected to spread the message effectively through school children to the adults. As describes GAP at least three road safety programs to be implemented in one district while ensuring women participation at least 30%. During the reporting period, conducted road safety public awareness programs for school children, motor cyclists and three wheel drivers.

4.2.2 Road safety awareness programs were based on some practical social safeguard requirements of iRoad program. Development of rural roads of iRoad program would be seen by villagers, as very new and attractive 'carpeted' roads enable them to drive/ride faster than earlier to save their time. At the same time, some pedestrians and villagers surrounding area may not feel differences on improved roads and its new conditions, but they use these roads as they used to do during the past. These psychological reasons emerge due to technical and physical changes in new roads, may lead to have unnecessary road accidents in future. In consideration with such assumptions, PIC2 assists RDA to implement safety awareness campaign complying with ADB safeguard guidelines as describe in 'Gender Action Plan' and 'Resettlement Framework'.

Table: 4.2.1 Road safety School Children Awareness Programs held during Reporting Period

Date	Package	Venue	No.of Participants	
			Male	Female
07.01.2019	KU-01	Kumbukkgahamulla School, Welipennagahamula	65	40
08.01.2019	AP-01	Kaluwila Sena Maha Vidyalaya	35	45
09.01.2019	PO-02	Minneriya National School	46	48
11.01.2019	AP-01	Wilachchiya Thakshila Maha Vidyalaya	34	48
17.01.2019	PO-02	Medirigiriya Maha Vidyalaya	54	43
06.02.2019	KU-02	Ketapathwehera Vidyalaya	45	60
07.02.2019	AP-03	Kurundankulama Maha Vidyalaya	45	62



Fig 4.2.2: 07.01.2019 KU-01 Program.



Fig 4.2.3: 08.01.2019 AP-01 Program Notice



Fig 4.2.4: 08.01.2019, RRDE address the students



Fig 4.2.5: 09.01.2019 PO2 Program, SGRS and Traffic Police officer address the students



Fig 4.2.6: 06.02.2019, KU-02 Program at Ketapathwehera Maha Vidyalaya





Fig: 4.2.7 The printing materials circulated among the public

4.2.3 School programs include a presentation on 'technical and social changes of rural roads after developing them through iRoad program' which is carried out by RRDE of PIC2 and road accidents and traffic rules and regulations by Traffic Police Officer. A leaflet is distributed among school children. Second program is meeting three wheel drivers and motorcyclists and distributing a leaflet with instructions on how to use new roads following traffic rules.



Fig 4.2.8: 07.01.2019 KU-01 program with three-wheel Drivers



Fig 4.2.9: 07.01.2019 KU-01 program with three-wheel Drivers



Fig 4.2.10: 07.01.2019 KU-01 program with Motor Cyclists in Office staff



Fig 4.2.11: 07.01.2019 KU-01 program with three-wheel Drivers



Fig 4.2.12: 07.02.19 AP-03 Program with female motorcyclists



Fig 4.2.13: 07.01.2019 KU-01 program with three-wheel Drivers

4.2.4 Programs for Kurunegala and Puttalam district are still not planned for other packages since their road construction is not completed. The majority of road safety programs in NCP have completed during end of 2018. KU-01 completed their awareness programs during reporting period and details are below.

Table: 4.2.1 Public Safety Awareness Program-Three-wheeler drivers/Motorcyclists

Package: RDA/ADB/iRoad(NW)/NCB/CP-RR(KU1)						
Road Id	Three wheelers			Motor cyclists		
	Number of three wheel parks visited	Number of three wheel drivers met	Number of drivers commented and records with photo graphs taken	Number of road users distributed leaflets	Number of cyclists from government offices visited circulated leaflets	Number of cyclist commented and records with photo graphs taken
30	1	5	0	10	0	0
32	0	10	0	10	1	0
33	1	15	1	15	0	0
34	1	15	0	15	0	0
36	1	15	1	10	0	0
47	2	25	1	30	0	0
48	1	20	1	15	3	2
49	1	10	0	10	0	1
50	1	25	0	30	5	0
55	1	20	0	25	0	0
76	0	20	0	15	0	0
81	1	15	0	20	75	1
82	0	10	0	10	0	0
86	1	20	0	15	1	0
97	1	15	0	10	0	1
98	0	10	0	10	0	0

4.3 Gender Status of Performance Based Maintenance (PBM)

4.3.1 GAP recommends that there should be 30% women participation in road maintenance as overall programe acheivements. Seven contract packages in NCP and two contract packages in NWP have already started PBM activities. However, the women participation shown 19.2% which is a considerable percentage and it could be expected slight increase in future when activities will be more in other contract packages. Table: 4.3.1 describe available data with their activities.

Table 4.3.1: Performance Based Maintenance (PBM)				
Package	Road IDs	Male	Female	Activities which are carrying out
AP-01	61,46,40,36,37,2,3,39, 43, 41, 38, 63, 45, 65, 12, 45	05	02	Vegetation control, tree branches pruning , Drainage cleaning
AP-02	19,23, 26, 27, 28, 47, 48, 49, 66, 67, 68,69	04	02	Vegetation control, tree branches pruning , Drainage cleaning
AP-03	5, 6, 71, 7, 24, 29, 70	03	-	Turfing, Removing debris, Drainage cleaning
AP-04	50,51,52, 53,55,56,57,59,16,17, 18,15	06	02	Grass cutting, Cleaning both sides of roads Vegetation control, Debris removing
KU-01	98, 81,76,30,82,34,33, 36,55,48,50,86,34,55,48,32, 47,97	30	02	Weed cutting Drainage cleaning
KU-02	56,66,75,58,67,56R,59L,45,43R, 57,102,74,5,1,100L,100K,4,43L	15	07	Weed cutting, Drainage Cutting
	<i>(No records available from Polonnaruwa)</i>	63	15	(Female 19.2%)

5.0 Monitoring of Social Safeguards Compliance at field level

5.1 Activities related to planning and implementation of road construction and compliance of social safeguard is key requirement of iRoad Program which is to be fulfilled by Contractors and it has to be ensured by PIC2. Overall supervision and State sector assistance are provided by the Employer. The performances of these three parties related to social safeguard compliance during the reporting period of January-June 2019 are briefed under the section five of this report while dividing in to sub sections such as PIU involvement in social safeguard monitoring, monitoring social safeguard activities in two projects, including involvements of the SGRS of PIC2, DSESAs, ESSO, CSD-Safeguard Consultant of ADB and ESDD-RDA. Practicing and outcome of Grievance Redress Mechanism (GRM) during the reporting period is also briefed.

5.2 Since the iRoad Program is basically a public participatory programme, field level monitoring of social safeguard compliances is compulsory. Program implementation is structured from the transect walks and GRC committee formation. In addition, public consultation on land donation and receiving consent letters and mitigating negative construction impacts during construction are sensitive activities which could not be handled by engineers alone. Hence the social and environmental staff was included in to the PIU, PIC2 and in the Contractors' packages. As in the past, field level monitoring was to be mainly focused on the delivery of the planned social safeguard activities for the affected people during planning, implementation and operational stages of the iRoad program. And also to ensure whether the planned activities are producing the desired outcomes as described by the Resettlement Framework (RF) of iRoad Program.

Key actors of field level monitoring are the PIC2, and two PIUs in NCP and NWP. In addition, ESDD of RDA and CSD/SG Specialist assist PIUs and PIC2 in building capacity of field level implementation staff and resolving social issues during program implementation. Following matrix summarizes relevant activities which fulfilled during the period of January-June 2019.

Table: 5.1 Key actors in social safeguard monitoring of iRoad Program, PIC2

Key Agency	Responsibility (As described in the RF)	Implementation Status
Two Project Implementation Units(PIUs) for two projects	<p>The PIUs will play the key role of coordinating with other concerned agencies and facilitating the entire process.</p> <p>Oversee land availability for taking up proposed roads. Implementation of land donation activities; signing MOUs and or receiving Consent letters. Ensuring GRCs have been formed and complaints are being addressed in a timely manner.</p>	<p>During the period of January-June 2019, proper coordination was continued by PIUs with relevant agencies, PRDA, CEB, Telecom, Mahaweli, Agrarian Services, Irrigation, DSO, Pradeshiya saba etc. (Pl. See the table 8.1,8.2)</p> <p>As required 236 level 2 and 84 level 3 GRCs in project 4 and 5 were formed during 2016-2018. (Pl. see the table 5.5.1)</p> <p>During the reporting period, there were 14 consent letters received from land donors. All together 875 consent letters have taken by two PIUs for additional land requirments. 875 is the cumulative figure at the end of June, 2019. (Pl. See the table 7.1)</p>
Project Implementation Consultants(PIC2)	<p>Review Level 1 design to ensure community suggestions have been integrated where feasible.</p> <p>Provide technical support and advice for addressing complaints and grievances and participate in resolving issues as a member of the GRC.</p> <p>Provide technical advice and on the job training to the Contractors as necessary.</p>	<p>Public requests which came through transect walks, public complaint register and GRC meetings have been incorporated in to final design. According to findings up to end of June 2019 are described in table 5.5.4</p> <p>During the reporting period SGRS, RE/ARE, DSESAs participated 03 level 2 GRCs and 02 level 3 GRCs which held for resolving public complaints during January-June 2019(See the table: 5.5.2).</p> <p>PIC2 facilitated to complete implementation of 14 HIV/AIDS public health awareness programs up to the reporting period. PIC2 carried out 10 progress review meetings which were helped to monitor field level social safeguard implementation during the reporting period and SGRS participated for 09 meetings. (Pl. See the table 5.1.1)</p> <p>SGRS has joined with other program team members to do field visits to resolve or investigate social issues in the field during the reporting period. SGRS has received public complaints directly over the phone during the period and attended with CEs, REs and ESOs to resolve them. SGRS of PIC2 prepare two Social safeguard reports once a year. The report include status of compliance with social safeguard requirments during the reporting</p>

	Preparation of semi-annual reports. Preparation of due diligence reports	period.Observations on activities of Employer, Contractor and Consultant are presents in these reports with recommendations. SGRS individually meet or contact ESOs in each package over the phone as and when necessary providing guidance how to implement and monitor and do reporting on the progress of implementation of social safeguard activities while complying with ADB guidelines.
Contractors	Ensure outcomes, suggestions from community consultations / transect walks are integrated into level 1 design. Commence constructions only when alignments free of encumbrance. Respond in a timely fashion to recommendations from GRCs.	During the reporting period ESOs and Design Engineers of Contractors have updated the study of public requests came through transect walks, GRC meetings and public complaint register and how many of them were incorporated in to final designs. Relevant records up to June 2019 have submitted to SGRS. Contractors' monthly progress review reports submitted during January -June 2019 included social safeguard progress during the period. PIC 2 reviews reports and data incorporate in to PIC2 monthly progress reports. All social data reported by Contractors are verified by SGRS with DSESAs.
ESDD	Conducting studies including social safeguard planning and socio-economic assessment for all future tranches.	During the reporting period of January-June 2019 there were some communication on GAP monitoring format and Gender Training programs.
CSD/SG Specialist	Assistance on CSD, social safeguard, gender compliance	CSD-Safeguard Specialist Consultant assisted the Employer and Consultant to resolve social issues and participated in several field visits and meetings. (See the table:5.4.1)

5.1 PIC2-SGRS' involvement in Social Safeguard activities during January-June 2019

5.1.1.1 As the SGRS of PIC2, carries out continues monitoring and reporting on Contractors activities and ESOs contribution in relation to social safeguard activities during the reporting period including traffic and safety management during construction, responding to public complaints and public requests and recording them, assisting PIU to carryout land donation activities, assisting to conduct GRC meetings to resolve public complaints etc. In addition to above safeguard monitoring activities, SGRS assists Contractors to implement social safeguard activities complying with contractual requirements. SGRS ensures proper coordination with the Social Safeguard Officers of PIUs and with the Resident Engineers' field level staff DSESAs, CEs, AREs, to resolve and clarify public issues as and when necessary during construction.

5.1.1 Implementation of Road Safety Public Awareness programs

5.1.1.2 Road safety awareness programs have been conducted as required in the GAP. These awareness programs are designed to make awareness at three levels (social categories) as school children and parents, three wheel drivers and motor cyclists. School level programs in packages KU1, AP1, PO-02, AP-01 and PO-02 during January 2019 and KU-02 and AP-03 during February 2019 completed within this reporting period. All these programs included two key lectures by Rural Road Design Engineer of PIC2, on 'rural road designs and the safety issues due to changes of the road conditions', and 'current status of road accidents in the area' by Traffic OIC of the closest main Police station. An introduction of the iRoad program is given by PIU representative and the objectives of awareness programs and social safeguard policies explained by the SGRS PIC-02 at the beginning of the programme.



Fig: 5.1.1 Inaugural session-Road Safety Public Awareness School Program 01(AP01, 08.01.19)



Fig: 5.1.2 Road Safety School Program AP01, (08.01.19)-Addressing by RRDE of PIC2

5.1.1.3 Another, two school programs completed during February 2019. One program was held on 6th February in KU-02, closer to RdId 67. About 100 school children and 50 parents of Ketapath wehera Vidyalaya participated. Other program was held in Kurundankulama School in AP-03. 168 students and 15 parents participated. During these programs PE of PIU-RDA provided an introduction on the iRoad program, secondly SGRS explained contractual and social safeguard requirement of having these programs. Thirdly RRDE-PIC2 (Rural Road Design Engineer) presented using multimedia presentation on new condition of rural roads after development and road safety requirements. Finally Senior Traffic Police Officer from the area presented current status of road accidents and the importance of knowing and following road safety rules, regulations and road signs.



Fig: 5.1.3 Reading Leaflets -Road Safety Public Awareness School Program 01 (AP01, 08.01.19)



Fig: 5.1.4 Safety Awareness campaigns for three wheel drivers-KU-01, 07.01.2019



Fig: 5.1.5 Safety Awareness building among women motorcyclists-KU-01, 07.01.2019

5.1.2 Assisting to resolve public social issues/Participating in GRC meetings

5.1.2.1 Road ID 29 (*Yakalla, Palugolla, and Upuldeniya Road*) in AP3 package passes over the bund of a minor village tank (*Mainly Siyabalagas Wewa and two other tank bunds; Karuwalagas wewa and Palugollewawa*). This minor irrigation tank belongs to the Agrarian Development Department (ADD) and there was a delay in receiving the concurrence from ADD for the proposed improvement works on this bund section of the road. This delay made the public living in the villages of Ihala Galkulama, Mooriyakadawala, Sivalakulama, Mailagas wewa, Himbutugollewa to development concerns about the proposed road development over this bund section. Several formal and informal meetings (even at the DS office) were held but the public kept on protesting. Finally a meeting was called with the public on 9th February 2019 with the participation of PD, PE, RE, ARE, CE, SGRS, SSO, DSESA, Assistant Agrarian Services Commissioner, OIC-Police, Buddhist monks, representatives of farmer organizations. The Assistant Commissioner of ADD explained construction laws/ policies related to development of tank bunds under ADD. Based on this explanation and considering the view of public it was agreed to macadamize the bund sections. This was accepted by the public, hence the issue was resolved.



Fig: 5.1.6 Public meeting on 09.02.19, AP-03 Rd 29 Tank Bund issue.



Fig: 5.1.7 High Priest explained their grievances at the meeting held on 09.02.19, AP-03 Rd 29 Tank Bund issue.

5.1.2.2 Second social issue from RdId-01 in KU-02, which was continuing during several months as unresolved, although have had several meetings and field visits. The issue was rain water flowing along RdId 01 of KU-02 diverting in to a Coconut estate after improvement of the road. Estate owner opposed the entering rain water in to their land and wrote to Chairman of RDA. Although there is a by- road and side drains, by-road users opposed sending water along the by-road. The meeting was held on 14th February 2019 in participation with PS-Chainman, PD-iRoad, RE, Land owners, SGRS, Villagers. No any positive solution came through the meeting, and fixed another meeting with more villagers on 25th February. Same participants and more villagers participated in 25th meeting and did a field visit and it was observed that strong rain water flow from iRoad developed road and damaging two houses close by. PD-PIU decided to divert water through build-up drains along the bi-road and villagers given their consent if build-up drain constructs. Also requested land owner to provide a strip of land to widen the bi-road for construction of build-up drains. Land owner given their consent and demarcated the land strip with the villagers. The issue was resolved.



Fig 5.1.8: 25.02.19 reaching solutions, KU-02, Rd 01 Drainage issue



Fig 5.1.9: 25.02.19 Meeting at the PS office on Rd 01 of KU-02 drainage issue.

5.1.2.3 Visited RdId 56 of KU-02 on 25th February to investigate an issue of delaying completing two house accesses. It was revealed that there is a workshop at this location and the owner had requested to extend the access at his cost. (ie. The Contractor to supply additional Hume pipe where the cost shall be

borne by the land owner.) However there has been a delay in supplying the Hume pipe. SGRS informed the contractor to expedite the action and complete the access.

5.1.2.4 SGRS visited RdId 33 of KU-03, on 21st February to inquire about a public complaint sent to the TL-PIC2. It was due to the delay of construction of the road and difficulties which people facing with incomplete road sections. Same complaint has gone to ADB-SLRM and CSD-Safeguard Consultant was also visited the road on 28th February. Then the complaints were explained by the SGRS and CSD-SG Consultant about the contractual issues of the contract package and it will be resolved soon and urgent unsafe sections will be improved.



Fig 5.1.10: 28.02.19 Contractor is not active and public are complaining, KU-03, Rd 33

5.1.2.4 A level 2 GRC meeting was held at RdId 87 of KU-03 on 5th March 2019, for clarification of social issues emerged due to delay of road construction by the contractor. PD, PE, RE, SGRS, CSD-Safeguard Consultant, GS participated. Public requested to complete road shoulders and some danger sections along the road and sections of bi-roads connecting. They questioned about the narrow road width. PD and CSD-Safeguard Consultant explained reasons to delay of construction and replied their questions referring iRoad program policies and reasons why rural road widening is not possible through the iRoad programme.



Fig: 5.1.11, A level 2 GRC meeting was held at RdId 87 of KU-03 on 5th March 2019



Fig: 5.1.12, Complaining on delay of Construction-RdId 87 of KU-03 on 5th March 2019

5.1.2.5 SGRS assisted the other GRC meeting which was held at RdId 67, of KU-02 on 13th March responding to a public complaint on some unclear issues of i Road construction of Rd.67. Complainer came to the meeting just before completion the meeting and he was unable to justify issues what he has written in the petition. Other villagers including PC member who participated, praised iRoad program and the quality of the road constructed. However, the complainer had some misunderstandings which were clarified by the PD.



Fig: 5.1.13, GRC meeting on RdId 67 of KU-02, Villagers appraise project activities

5.1.2.6 A Level 3 GRC meeting was held at RdId 27 of KU-04 on 30th March 2019 due to public unrest on the delay of construction of the road. Since the PIU had held a similar GRC meeting, a year ago to discuss the same issues which public were complained, PD explained the contractor was unable to keep promises and contractor is responsible in expediting construction. Then the contractor promised again to complete this road within a year. People were unhappy with the contractor and also with the RDA. PD explained the actual situation and finally people requested RDA to do temporary repairs to the road section close to the school which New Year festival will take place. Contractor promised to do temporary works and also promised to complete the road construction during November 2019.



Fig: 5.1.14, 14.03.2019, KU-02 Drainage Issue, Field Inquiry by PS Chairmen, PD-NWP, Land Owner



Fig: 5.1.15, 14.03.2019, KU-02 Drainage Issue, Looking at alternative water paths by RRDE and CSD-Safeguard Consultant

5.1.2.7 Responding to public complaints, SGRS visited some roads in KU-01 and PU-03 on 2nd April 2019. Among the roads visited were Rd.Id22 and 24 of PU-03. Issues which affect to road users were observed such as partly constructed roads including without having proper road shoulders which causing road accidents. Road 33 of KU-01 was also visited. The DSESA also joined with SGRS to visit roads. Findings submitted to Resident Engineer in writing for providing necessary instructions to Project Manager of the Contractor to rectify them as soon as possible. Also visited road IDs 24, 17, 22, 38, 39 and 40 and found safety issues as a result of incomplete road construction. Most of roads were asphalted but not shoulders completed. Some culverts were partly constructed and some were not widened as planned. Gravel area which getting mud during rainy days due to incomplete road shoulders. All findings informed to the RE-Puttalam.

5.1.2.8 SGRS joined with the PD, PE, RE and PM on 24th May 2019, to resolve a public complaint on water stagnation due to a lead away issue at a road section in the road Id 02 of KU-02, developed through iRoad programe. Land owner was not willing to open existing lead away through his land. The Chairmen of Pradeshiya Saba cut forcefuly a drain in this land but it was not agreeable with iRoad program

policies. PD informed the land owner, if he gives consent in writing to allow the lead away through his land the culvert will be constructed at the road. Then the land owner wanted a build-up drain and PD stated that it was not possible to do construction outside the road due to limited funds. At that meeting (24th May) the problem was not resolved. RE of the Consultant decided to concrete the road section.



Fig: 5.1.16, Drainage Issue KU-02, RdId 02, field investigation by PD, PE, RE, SGRS, PM on 24th May

5.1.2.9 SGRS attended in another public complaint resolving meeting, complained by a Funeral Assistance Society at RdId 01, PU-01 on May 2019. Complainer has mentioned that the cattle rearing people in the area damaging the road during bringing cattle along the newly constructed road sections. Also the road traffic will be bad especially for school going children at that time. SGRS suggested the TL-PIC2, having a GRC meeting will be suitable and informed to the PD-NWP. SGRS, PD, PE, RE, PM met the Divisional Secretary on 23rd May and decided to have a GRC meeting on 4th June at the Anamaduwa DSO with relevant parties. A level 2 GRC meeting was held on 4th June to resolve a public complaint and the DS of Anamaduwa, AD Planning, and the PD-iRoad have participated in the meeting. However the complainant was not there. PD-iRoad suggested the cattle farmer to use limited locations to direct cattle to cross the road and minimize damages to the road. Villagers who participated in the meeting mentioned that it was not serious social issue in the area or to road sections although an unknown person has sent several petitions against cattle farmers.

5.1.2.10 The PD-NWP complained SGRS on 23rd May 2019, the RdId 12 in PU-01 has been closed for the public due to construction of a road section, without informing the PD or RE or any other government agency at the road in advance, including Navy Camp. The Chief Navy Officer had complained to the PD on their difficulties. SGRS informed the TL-PIC2 and TL instructed the contractor to stop temporary construction and inform relevant institutions including Navy Camp on road closing schedules in advance and then do construction. Following day everything was settled and contractor informed the public following proper way and construction continued.

5.1.2.11 SGRS participated in a level 2 GRC meeting held on 28th May 2019 with the PD, PE, RE, ARE, ESO-PIU, Engineer of Dept of Irrigation and PM of Contractor at a community hall in RdId 7 of package PU-01. A similar GRC meeting has been held about six months ago to resolve technical problems related to the quality and road construction methods which were raising by some village people continuously. The same discussed at this (28th May) meeting again since the contractor was not able to rectify previous issues. Erosion of road shoulder sections, embankment failures, weak asphalted sections, cracks in the some asphalted sections, lack of house access etc. were the complaints. PM of the contractor explained the reasons to delay of rectifying the same issues discussed during last year. The contractor has shifted to other roads without completing this road and he informed that after 5th of July they will start to rectify all issues in this road.



Fig 5.1.17: 28.05.2019 GRC meeting-Field visit with villagers to see construction issues,PU-01,RdId 7



Fig 5.1.18: 28.05.2019 GRC meeting to resolve public complaints held for RdId 07,PU1



Fig 5.1.19: 28.05.2019 Contractor of PU-01 explained how and when rectify construction defects of RdId01



Fig 5.1.19: Anamaduwa DS office meeting to resolve public complaint on cattle movements along new road

5.1.3 Monthly Progress Review Meetings (MPRM)

5.1.3.1 The Consultant organizes monthly progress review meetings with the Contractor and the Employer to review the progress of construction works, issues and find solutions. The issues discussed at MPRMs of NCP and NWP during the reporting period are briefed in short here in order to months of MPRM held from January to June 2019. Conducting MPRMs for Anuradhapura and Polonnaruwa during reporting period was limited, only two meetings have had, since the most of roads were completed and handed over. Two MPRM meetings were held on 10th January and 22nd February 2019. Issues discussed at these two meetings were, the balance five roads to be completed in the AP-01, the house access issues to be resolved at RdId 10 and 01, completing RdId 59, 48 and 49 of AP-02 etc. SGRS reminded that as the Gender Action Plan recommends women participation is necessary during (PBM) road maintenance period. Also SGRS reminded AP-02 to have road safety school programs during March. RE reminded to resolve house access before handing over roads. Since KU-03 and KU-05 contract were terminated no MPRM held for them during the reporting period in Kurunegala district.



Fig 5.1.20: MPRM-Anuradhapura, discussions on remaining issues

5.1.3.2 MPRM for February in Puttalam was held on 25th February. It was reported that the progress of three packages was very unsatisfactory. Informed that the PU-02 PM has resigned from the post. Nobody who responsible for PU-01, did not participate in the meeting. PU-03 activities were not sufficient to reach targets and the TL asked all PMs to ensure road safety and control of environmental issues of partly completed and abundant roads.

Table 5.1.1: MPRMs held during reporting Period in NCP and NWP

Date	District	Packages
09.01.2019	Puttalam	PU-01,PU-02,PU-03
10.01.2019	Anuradapura	AP-01,AP-02,AP-03,AP-04
22.02.2019	Anuradhapura	AP-01,AP-02, AP-03, AP-04
25.02.2019	Puttalam	PU-01,PU-02,PU-03
13.03.2019	Kurunegala	KU-01,KU-02,KU-04
27.03.2019	Puttalam	PU-01,PU-02,PU-03
03.04.2019	Puttalam	PU-01,PU-02,PU-03
04.04.2019	Kurunegala	KU-01,KU-02,KU-04
16.05.2019	Kurunegala	KU-01,KU-02,KU-04
18.06.2019	Puttalam	PU-01,PU-02,PU-03

5.1.3.3 Progress reviews carried out with KU-01, KU-02 and KU-04 on 13th March 2019. A road maintenance reporting format to be introduced to KU-02 and KU-01 by SGRS since requirement of complying with GAP of ADB guidelines. Crack survey issues in road Id 66 of KU-02 were discussed. Also SGRS informed ESOs to have proper system for recording pre- crack survey information and to follow KU-01 pre-crack survey format.



Fig 5.1.21: MPRM-Kurunegala District, 04.04.19

5.1.3.4 Enother Puttalam district progress review meeting was held on 27th March. Earth work delay of RdId 12, Irrigation canal issue at Rd 43, and remaining issues at RdId 14 and 15 were discussed as issues of PU-01. TL instructed the PM to take actions to resolve and expedite works. PU-02 did not show any progress of activities than the last month. Payments delay for workers of PU-03 was discussed. PM promised to pay before 3rd April 2019.



Fig 5.1.22: 25.02.2019 PU-02 Progress Review Meeting



Fig 5.1.23: MPRM Puttalam District, PU-03, 03.04.19, Poor Progress of construction activities

5.1.3.5 SGRS participated in MPRM held for three packages of Puttalam District on 3rd April 2019. During discussing the progress of PU-01, PM stated that the RdId 44 and 8 were ready to handover. Also discussed some issues such as cracks in a wall, cutting a Bo-Tree and regarding a causeway at RdId 05,. Decided to call a GRC meeting to resolve the causeway issue. It was also discussed the issue of delaying payments salaries to office and field staff members by the contractor of PU-02 and PU-03. PM promised to pay March salary before 10th April. PD stated that RdId 9, 6 and 4 are not in motorable conditions due to incomplete road shoulders.

5.1.3.6 MPRM for Kurunegala district held on 4th April. The importance of doing pre-crack survey was explained to KU-04 staff. Also asked the PM of KU-04 to keep promises which given at the GRC on RdId 27, before the New Year. Workers' salary payments to be paid as scheduled. Due to improper crack survey records a public complaint is becoming critical at Rd.66 of KU-02.TL instructed the PM to resolve the issue.



Fig 5.1.24: 18.06.2019 MPRM, PU-03, How to improve the progress

5.1.3.7 MPRM for the month of May 2019 for Kurunegala district held on 16th May. Progress of KU-02 and KU-04 reviewed. Discussed the findings of SGRS' field visit on 8th May in some roads in KU-04 with the ESO of the Contractor. RdIds 7, 8, 27 and 28 visited and some safety and environmental issues were

noted and SGRS asked the ESO to rectify with relevant officers. SGRS mentioned at the MPRM that , the contractor had promised to public at the GRC meetings held in last year to complete RdId27, but still not done. Contractor informed that 2-3 Km road section and some culverts have already constructed.



Fig 5.1.25: 18.06.2019 PU-01 Progress Review, to improve safety, environmental safeguard

5.1.3.8 MPRMs for June 2019 for Puttalam and Kurunegala districts were held on 18th and 20th June 2019. SGRS participated in MPRM of Puttalam district. Poor construction performances in PU-03 and lack of construction activities in PU-02 were reported. A new PM for PU-03 was introduced, and he will expedite activities. Although the PU-01 progress was satisfactory, it was reported that some social environmental safeguard issues were still remaining to resolve. Team Leader advised the PM to rectify them. Also the TL mentioned that the importance of complying with Labour Laws in paying monthly salaries and contributing EPF, ETF etc. related to workers of contractors are very important.

5.1.4 Monitoring Land Donation

5.1.4.1 SGRS assists PIUs to investigate land donation requirements and monitor such activities ensuring compliance of Land Donation policies described in the RF. During the reporting period there were only 14 consent letters received by two PIUs for additional land requirements. Cumulative figure of total consent letters so far received is 875. Since the most of roads are now completing in NCP and two packages in Kurunegala are terminated. SGRS joined with PIU-SSO and ESO of NWP on 26th February to investigate land donation requirement of KU-04. There was a request from the contractor to divert rain water and seepage water coming from the tank bund, to an outside land at RdId 07. Land owner was willing to donate land strip. Other requirement was at RdId 08 in Giribawa area. The need of additional land strip has created from a newly constructed culvert for controlling inundation during heavy rain however there was not a proper lead away, except the adjoining home garden. It was found that the land owner Mr.Gunaratne was happy to provide a land strip through their home garden. Then the Consent letter was signed.



Fig 5.1.26: Mr.Saman Gunaratne of Warawewa liked to donate land strip for a lead away, KU-04,Rd 08

5.1.4.2 Another additional land requirement was investigated and consulted land owners who willing to donate, on 21st March with PIU-NWP ESOs, and CE at RdId 17 of PU-03. Land owner voluntarily allowed their land strip to use as a lead away drain.



Fig 5.1.27: Mr.Saman Gunaratne signing Consent Letter with his wife, RdId 08, KU-04

5.1.5 Updating the PPMS

5.1.5.1 Complying a project requirement, Project Performance Monitoring report (PPMS) has been updated during January 2019 and the social section was completed by SGRS. To collect some more details and update available information on 'rural hubs', to include in the PPMS, SGRS did field visits during 16th, 17th, and 18th in Kurunegala, Puttalam, Anuradhapura districts.

5.1.6 Public Health Awareness Program

5.1.6.1 Complying with Gender Action Plan, Public health and HIV/AIDS awareness program has been implemented in all other 13 Packages except PU-03 and AP-03. Information on other programs were included in the previous semi-annual report. However, AP-03 program was successfully organized and implemented by MAGA on 24th January 2019. It was held at Palugaswewa DS office with the participation of 101 villagers, and 60 males and, 41 females have participated.



Fig 5.1.28: Public Health Awareness Program (AP03, 24.01.19)-Lecture by Medical Officer



Fig 5.1.29: Public Health Awareness Program (AP03, 24.01.19)-Participants from villages

5.1.7 House Cracks Investigation

5.1.7.1 There were several complaints on cases related to house cracks, observed by the SGRS during the reporting period.

5.1.7.2 One complaint received by SGRS on 14th March 2019 from the TL-PIC2, which was sent by one Ms.Ashoka Kumari from RdId 27, KU-04, mentioning that her house walls and the roof have damaged due to road construction. SGRS investigated the complaint. The house was in dilapidated conditions. There were cracks, however the contractor has not kept pre-crack survey records, and then instructed the contractor to negotiate with the complainer and pay a reasonable compensation.

5.1.7.3 Other complaint received by SGRS from the RE-KU, on 2nd April 2019, related to house cracks due to construction of Rd.66 of KU-02. During investigations, revealed that no proper method has used to do pre-cracks survey by the contractor. Contractor has written to the insurance company and waiting for their response.

5.1.7.4 The other public complaint on house cracks which might have caused due to construction of RdId 41 of KU-02, investigated on 19th June 2019. The road section, in front of the complainer's house has completed during September 2018. Pre-crack survey records indicated that there were cracks in the wall before road construction. However, some cracks were in the floor were not recorded through

the pre-crack survey. Contractor suggest to do an investigation with the officers of the Insurance agency and decide the solution.



Fig 5.1.30: KU-04, RdId. 27Ms.H.M.Ashokakumari's dilapidated house found wall cracks

5.1.8 Social Safeguard Issues in terminated/Poor progress contract packages

5.1.8.1 During the reporting period, KU-03 and KU-05 contract packages were terminated by the employer considering Consultant's recommendations. However there were partly constructed road sections with uncompleted culverts, drains, bridges, and ABC laid road sections which create safety problems to road users.



Fig: 5.1.31, KU-03 RdId 22 one of the roads abundant with unsafe conditions after termination

5.1.8.2 It has been identified by the Consultant the locations in the table to be improved at least temporary, until appoint new contractors considering safety of road users.

Package	Road IDs	Locations
KU-03	22,20, 23, 84,37, 68	1+502, 1+590, 2+871, 3+290, 6+500, 10+267, 14+965, 15+776, 16+023 of Rd.22, 4+830, 4+880, 4+290, 4+550 of Rd.20, 0+071, 4+515, 7+914, 0+619, 7+914 of Rd.23, 2+620 of Rd.84, 1+342, 1+260, 3+113 of RdId 37, 0+360, 3+075, 0+010, 0+075 of 68
KU-05	RdIds 99, 88, 83, 89, 13, 70, 12, 29, 79	Not available

Above roads are in contract package KU-03 and KU-05 were already terminated, however to be developed considering safety risks until new contractor will be mobilized. MAGA contractor was given a contract to complete partly constructed activities in KU-03 and KU-05 and photographs below show the previous situation and conditions after improvements.



Fig 5.1.32: RdId 22, KU-03, Uncompleted construction of the bridge created safety issue to school children



Fig 5.1.32: RdID 22, KU-03 after bridge Construction-Ensuring Safety of School children and others



Fig 5.1.33: RdID 22, KU-03 Dangerous road sections were improved ensuring road safety of the road users.



Fig 5.1.34: RdID 22, KU-03 After providing with safety measures



Fig 5.1.35: RdID 22, KU-03 After providing necessary safety measures



Fig 5.1.36: RdID 22, KU-03
incomplete construction of the bridge



Fig 5.1.37: RdID 22, KU-03 After completion of structure by
MAGA contractor




5.1.9.3 In addition to roads in terminated contract packages construction of slow progress contract packages were also visited SGRS with ESOs and Construction Engineers. RdIds 27, 28 of KU-04, other roads in PU-02 and PU-03 visited and identified safety issues and informed the ESO and Safety officer to rectify them.

5.2 District Social and Environmental Assistants (DSESAs) Social safeguard Activities






DSESAs Involvements in complying social safeguard policies during reporting period are briefed below. (From January to June 2019) Some examples from their activities have carried out by DSESAs in Kurunegala and Anuradapura have included in the matrix with some photographs. Since no programme activities carried out during reporting period by the DSESA in Polonnaruwa due to completion of road construction, no issues reported to the semi-annual report. Also the descriptions on special social safeguard issues have not reported by DSESA in Puttalam.






Table: 5.2.1






DSESAs (KU) Involvements in complying social safeguard policies during implementation I Road program





Date	Activity taken place- Venue/ Package/ Road	Activity involved/conducted	Outcome related to Social Safeguard Compliance	Remarks(Photographs)
02/01/2019	KU1 (RD ID 81)	Attending to public request for a hume pipe access 02/01/2019	Informed CE and he emphasized that a hume pipe access was not required to that location. Lady was aware kindly through CE on that day(02/01/2019)	
04/01/2019	KU2 (RD ID 74) at Ch.2+372 LHS	Attending to public complaint- Overflowing of rain water towards garage from road on 11/09/2018	Involved to provide a proper culvert with house access discussing with ARE. Now the problem has solved (04/01/2019)	<div>Before</div>  <div>After</div> 


04/01/2019	KU2 (RD ID 74) at 1+900 LHS	Observed debris that can be harmful to residencies on 28/11/2018	Informed contractor to clean the location as soon as possible- That place had been cleaned by the contractor on 04/01/2019	 <p>Before</p>  <p>After</p>
23/01/2019	KU 02 (RD ID 67)	Attending public complaint about damaged by road due to contractor's vehicles on 23/01/2019	Discussed with, PD, PE, ARE, SGRS, PM and villagers. It was stated that by road has not been damaged due to contractor's vehicle. villagers accepted it at the GRC meeting held on 13/03/2019	 <p>During inspection that place with PE</p>  <p>During discussion with villagers at GRC meeting</p>
29/01/2019	KU 01 (RD ID 98) Ch. 0+300 RHS	Involved in road Handing over inspection -Highlighted a Issue about lead away blocking which carry runoff water to near tank cc)	Gathered agrarian development officer and villagers and discussed the issue with villagers. At that time construction was stopped at the location and this problem was directed to Pradeshiya sabha to resolve coordinating with Agrarian department and Irrigation department on 29/01/2019	 <p>During inspection with Pradeshiya sabha</p>





				 <p>During meeting with Agrarian Development officer</p>
29/01/2019	KU 01 (RD IDs 98) Ch.2+200	Involved in road Handing over inspection - Monitored debris on either side of the roads also within the road that create safety risks.	Informed contactor to remove debris/access material on either side of the roads and inside of the road for avoid creating social problems on 13/11/2018. Road had been cleaned on 29/01/2019	 <p>Before</p>  <p>After</p>  <p>During handing over inspection</p>
06/01/2019	KU 02	Participated in road safety awareness program for school children on 06/01/2019.	Contributed to aware school children about road safety practices when using roads.	 <p>During Awareness program</p>



25/02/2019	KU 02 (RD ID 56) 6+000 RHS	Involved to road Hand over inspection -recorded non reinstate stock yard until road hand over 25/02/2019.	Informed contractor to reinstate stock and disposal yards before rising public grievances on 25/02/2019. Problem had been solved and satisfaction letter of the land owner had been taken on 23/06/2019.	 <p>View of the yard observed</p>
01/03/2019	KU 02 (RD ID 01) 3+200 RHS	Attending to public complaint of Mr.George Fernando about a washing out his land	Inspected with Engineers' team and they approved a continuous U drain system with proper access on to that location on 05/09/2018. Approved U drain system with proper access had been provided on 01/03/2019.	 <p>Before</p>  <p>After</p>
05/03/2019	KU 03 (RD ID 87) 0+825-1+800	Participated in GRC meeting to discuss about poor road maintenance on 05/03/2019.	PD informed villagers about current situation of the contract package and agreed to maintain the road. Maintenance have been done during end of the March on 2019	 <p>Discussing maintenance issue at GRC meeting</p>  <p>During Maintenance</p>

13/03/2019	KU 02 (RD ID 67)	Participated in GRC meeting to discuss about problem of completing the handed over road on 13/03/2019.	Listened to the grievances and all problems were clarified by i road team discussing with villagers. The people who aggressively talked clear about issues after explaining by PD-NWP and CSD-SG consultant who described project's policies.	 <p>During GRC meeting</p>
14/03/2019	KU 02 (RD ID 01) 12+663 RHS	Attending in public complaint to discuss about lead away problem and discussed to provide a drainage path as needed.	Drainage path has been built 90% resolving issues. This construction to be completed at the end of the August 2019.	 <p>Observing the issue</p>
27/03/2019	KU 02 (RD ID 75 0+000 to 3+388)	Involved in road Handing Over inspection -observed safety issues as debris/access material on either side of the roads and inside of the road.	Informed the contractor to clean the site to avoid social issues at 0+000 to 3+388 on 27/03/2019. Debris had been removed on 06/06/2019.	 <p>Observed issues</p>  <p>During handing over inspection</p>
19/03/2019	KU 02 (RD ID 100)	Involved in road Handing over inspection -Highlighted the access to be provided at 2+220 RHS that.	Approved a pre cast box culvert / access. Pre cast box/ access to be provided end of August 2019.	

25/03/2019	KU 02 (RD ID 66) 3+520 RHS	Contractor has been informed to close the house crack problem with house owner Mrs.Thamara Kumari on 12/12/2018 During In road Handing over inspection on 25/03/2019 I had to monitor this crack complaint.	Informed contractor again. Now insurance company has involved and follow the insurance process for compensation.	
26/03/2019	KU 02 (RD ID 41)	Monitoring safety in construction sites	Informed contractor about poor safety of the site and workers on 26/03/2019. And informed CR to Provide PPE for labours and supervisors. PPE and site safety had been improved on 20/04/2019.	 Before  After
29/03/2019	KU 02 (RD ID 58) 5+520 LHS	Involved in road Handing over inspection -Highlighted an unsafe telephone post.	Informed RDA about the unsafe telephone post, then RDA Informed relevant agencies to remove this telephone post. Telephone post had been shifted.	

02/04/2019	KU 04 (RD ID 27)	Participated in GRC meeting to discuss about poor conditions of the road	<p>Listened to the their grievances about poor road maintenance and agreed to maintain road before new year. Road maintenance was done to fulfill their requirements before new year season.(10/04/2019)</p>	 <p>During GRC meeting</p>  <p>During maintenance</p>  <p>Maintained road</p>
05/04/2019	KU 02 (RD ID 41) 1+750 RHS	<p>Attended in a public request of Mrs. Vijayasundara 's request on a protection wall for her land on 01/10/2018</p>	<p>After informing Engineers they approved a toe wall at 1+750 RHS. It had been constructed on 05/04/2019</p>	 <p>During inspection</p>  <p>Constructed toe wall for protect the land</p>

05/04/2019	KU 02 (RD ID 41) 3+840- 3+845 LHS	Attended in a public request of Mr. P.R. Chaminda Kumara for a protection wall for his land on 01/10/2018	After informing Engineers they approved a retaining wall 3+840-3+845 LHS . It had been constructed on 05/04/2019	 <p>Inspection with ESO on 01/10/2018</p>  <p>Unstable Embankment on 01/10/2018 3+840-3+845 LHS</p>  <p>After Construction of retaining wall on 05/04/2019</p>
08/04/2019	KU 04 (RD ID 92)	Attend in public requests of villagers for new culverts.	Villagers were aware through ESO about project scope and policies, and CE stated that no needed new culverts.	

17/06/2019	KU 04 (RD ID 92) 2+300 LHS	Attended in complaint of broken fence; damaged by contractor's vehicles on 27/03/2019	Fence had been repaired on 10/06/2019. As well as a satisfaction letter of land owner had been taken by ESO after solving problem.	 <p>Before</p>  <p>After</p>
<i>Prepared by DSESA-Kurunegala District</i>				

5.2.1 DSESA-Anuradhapura district Social Safeguard Activities-Resident Engineers Office				
Date	Activity taken place-Venue/Package/Road	Activity involved/conducted	Outcome related to Social Safeguard Compliance	Remarks
2019/01/08	AP01 Sena Maha Vidyalaya, Hidogama	Conducted by Maga engineering AP1 package on road safety awareness for school children and parents	To provide awareness on road safety for children and parents also. Road accidents, road safety rules, road signs, and what are the things that we have to consider when we use new roads.	School principle, teachers and children were very thankful with such a program and they satated that they received more information on road safety.
2019/01/11	AP01 Thanksila Maha Vidyalaya, Villachchiya	Conducted by Maga engineering AP1 package. Regarding road safety awareness for school children and parents	To give awareness on road safety for children and parents also. Road accidents, road safety rules, road signs, and what are the things that we have to consider when we use the road very carefully.	School principle, teachers and children were very thankful with such a program and they satated that they received more information on road safety.
2019/02/07	AP03/Kurundam Kulama Vidyalaya	Maga Engineering (Pvt) Ltd, to the school children, teachers & parents	To ensure the road improvements in the project area, provide a long-term safe environment for road users and local inhabitants, and contribute strongly to the overall reduction of road accidents elsewhere in Anuradhapura District. To achieve this, both community-based education and enforcement measures will be required. To reduce the loss of life & property	School principle, teachers and children were very thankful with such a program and they satated that they received more information on road safety.

			damage caused by road accident To reduce unnecessary medical cost. To give knowledge for school community about how accident is occurred.	
<i>Prepared by DSESA-Anuradapura</i>				

5.3 PIU involvement in social safeguard implementation and monitoring

5.3.1 As the Resettlement Framework describes, facilitating formation of Grievance Redress Committees (GRCs) with the DS Offices and formation of level two GRCs with villagers of the roads are to be developed, resolving social issues using GRC committees are important tasks being implemented by PIUs in two projects. Accepting donation of lands to improve road conditions which are coming as voluntary requests of villagers is also an important legal activity which PIUs are taking responsibility with Divisional Secretaries. Project Director is in each province assisted by Project Engineers and Social and Environment Safeguard officers to implement above activities. Participating in monthly progress review meetings (MPRM) and reporting PIUs observation or forwarding public complaints at the meeting continued during the reporting period and PIUs participated in 10 MPRMs. Since the road construction in NCP almost completed, there were only two MPRMs held in Anuradapura. No any MPRM held in Polonnaruwa since all roads were handed over during end of 2018. However coordination on project activities between the consultant and PIUs have continued by REs, PEs, CEs, SGRS, ES, and AREs during reporting period.

5.3.2 Project 4, NCP activities during the period were very less in relation to social safeguard since the majority of roads of seven packages completed and handed over. However, responding public complaints during road handing over and coordination with line agencies continued during the reporting period from January to June 2019. Social Safeguard Officer (SSO) and Environmental Safeguard Officer (ESO) carried out Land Donation and investigation and resolving public requests and complaints in coordination with the Consultant. PIU-SSO's involvement during the reporting period are briefed below.

Table: 5.3.1 PIU-NCP activities

PIU-NCP, SSOs Social safeguard Activities during January-June 2019				
Date	Activity /Purpose (Meeting /field visit etc.)	Package/road/ chainage/ venue	PIU Members who participate	Out come
2-Jan-19	Handing over	PO3/71	PE/ESSD	Identified issues have forwarded to the consultant and consultant provided instructions to the contractor.
7-Jan-19	Handing over	AP2/47	PE/ESSD	
9-Jan-19	Handing over	AP2/47	PE/ESSD	
10-Jan-19	GRC Meeting	AP3/29	PD/PE/ESSD	Discuss with related Authority and peoples will receive the solution.

16-Jan-19	Handing over	AP3/09	PE/ESSD	Identified issues have forwarded to the consultant and consultant provided instructions to the contractor.
17-Jan-19	Handing over	AP3/08	PD/PE/ESSD	
18-Jan-19	Handing over	AP1/12	PE/ESSD	
21-Jan-19	Handing over	AP3/60	PE/ESSD	
23-Jan-19	Site inspection	PO2/73	ESSD	Obtained required land space to road construction
24-Jan-19	Handing over	AP2/48	PE/ESSD	Identified issues were forwarded to the consultant and consultant provided instructions to the contractor.
29-Jan-19	Handing over	AP1/43	PE/ESSD	
30-Jan-19	Handing over	AP1/41	PE/ESSD	
31-Jan-19	Handing over	AP2/67	PD/PE/ESSD	
1-Feb-19	Handing over	AP1/40	PE/ESSD	
2-Feb-19	Site inspection	AP3/29	PD/PE/ESSD	
5-Feb-19	Site inspection	AP3	ESSD	
6-Feb-19	Handing over	AP1/36	PE/ESSD	
7-Feb-19	safety awareness	AP3	ESSD	
9-Feb-19	Site inspection	AP3/29	ESSD	Eexplained present project situation-Contractors' issues
11-Feb-19	DCC Meeting	AP4/Padaviya	ESSD	
13-Feb-19	Site inspection	AP2/69,68	ESSD	
18-Feb-19	Site inspection	AP1	ESSD	
26-Feb-19	Site inspection	AP1/AP4	ESSD	Identified issues were forwarded to the consultant and consultant provided instructions to the contractor.
1-Mar-19	Site inspection	AP1/36	ESSD	
2-Mar-19	Site inspection	AP3	ESSD	
4-Mar-19	Site inspection	AP1	ESSD	
8-Mar-19	Site inspection	AP3/29	ESSD	
11-Mar-19	Site inspection	AP1	ESSD	
12-Mar-19	Site inspection	AP3/60	ESSD	
13-Mar-19	Handing over	AP1/36	PE/ESSD	
14-Mar-19	Site inspection	AP3/30	ESSD	To aware the people about the project. Explained present project situation
17-Mar-19	Opening ceremony	Anuradhapura	PD/PE/ESSD	
18-Mar-19	DCC Meeting	AP4/Padaviya	ESSD	Explained present project situation
25-Mar-19	Site inspection	AP1/36	ESSD	Identified issues were forwarded to the consultant and consultant provided instructions to the contractor.
27-Mar-19	Site inspection	AP1	ESSD	
28-Mar-19	Site inspection	PO1	ESSD	
4-Apr-19	Handing over	AP3/71	PE/ESSD	

5-Apr-19	Site inspection	AP1/AP3	ESSD	
6-Apr-19	Site inspection	AP3	ESSD	
8-Apr-19	Site inspection	AP3	ESSD	
10-Apr-19	Handing over	AP3/60	PE/ESSD	
11-Apr-19	Site inspection	AP4	ESSD	
23-Apr-19	Site inspection	AP1/36	ESSD	
25-Apr-19	Handing over	AP3/60	PE/ESSD	
29-Apr-19	Site inspection	AP1/36	ESSD	
2-May-19	Site inspection	AP1/36	ESSD	
4-May-19	Handing over	AP3/71	PE/ESSD	
5-May-19	Site inspection	AP1/AP3	ESSD	
6-May-19	Site inspection	AP3	ESSD	
8-May-19	Site inspection	AP3	ESSD	
10-May-19	Handing over	AP3/60	PE/ESSD	
11-May-19	Site inspection	AP4	ESSD	
23-May-19	Site inspection	AP1/36	ESSD	
25-May-19	Handing over	AP3/60	PE/ESSD	
29-May-19	Site inspection	AP1/36	ESSD	Identified issues were forwarded to the consultant and consultant provided instructions to the contractor.
8-Jun-19	Site inspection	AP3/29	ESSD	
12-Jun-19	Site inspection	AP1/43	ESSD	
18-Jun-19	Site inspection	AP2/67	ESSD	
20-Jun-19	Handing over	AP1/10	PD/PE/ESSD	
22-Jun-19	Site inspection	AP3/29	ESSD	
25-Jun-19	Site inspection	AP3/29	ESSD	
26-Jun-19	Site inspection	AP3/09	ESSD	
29-Jun-19	Site inspection	AP1	ESSD	



Fig: 5.3.1 SSO signing Consent letters for land donation



Fig: 5.3.2 Roads opening ceremony with the PD-PIU, NCP

5.3.2 NWP Safeguard activities involved during reporting period ESO and AESO.

Date	Activity	Package/ Road/ Venue	PIU members	Outcome
January				
08	Road inspection	KU02 ID 101	PE, ESO,AESO	Inspected land donation places.
09	GRC meeting, Mahawewa DS office	PU03 ID 21	PD, PE, ESO,AESO	Discused the problems regarding ID 21 and problem solved.
28	Road safety inspection	PU 01 ID 8 & 43	PE, ESO,AESO	Inspected about road safety regarding construction works.
31	Steering committee meeting	NWP	PD, PEs, ESO,AESO	Coordinated iRoad project, NWP with other government authorities.

February				
25	GRC meeting	KU01 ID 01	PD,PE,ESO,AESO	Discussed the problems regarding ID 01 and problem solved.
26	Land donation	KU04 ID 07 & 08	ESO,AESO	Consent letters are signed.
28	Complaint inspection	KU03 ID 22	PD, PE, ESO, AESO	Discussed the complaint regarding ID 22 and problem solved.
March				
01	Road inspection	KU02 ID 01	PE, ESO, AESO	Inspection for Environment, social and safety issues.
05	GRC meeting	KU02 ID 87	PD, PE, ESO, AESO	Discussed the problems regarding ID 87 and problem solved.
06	Road inspection / Complaint inspection	KU 02 ID 67	PD, PE, ESO, AESO	Discussed the problems regarding ID 67 and problem solved.
13	GRC meeting	KU 02 ID 67	PD, PE, ESO, AESO	Discussed the problems regarding ID 67 and problem solved.
	Crack inspection	KU 02 ID 66		Inspected cracked house in ID 66.
14	Road inspection	KU02 ID 01	PE, ESO, AESO	Inspection for Environment, social and safety issues.
26	Road inspection	KU03 ID 37 & 68	PE, ESO, AESO	Inspection for Environment, social and safety issues.
27	Land Donation	KU 04 ID 07	PE, ESO, AESO	Consent letters are signed.
	Public complaint inspection	KU 04 ID 90		Discussed the problems regarding ID 90 and problem solved.
30	GRC meeting	KU04 ID 27	PD, PE, ESO, AESO	Discussed the problems regarding ID 27 and problem solved.
May				
06	Culvert inspection	PU01 ID 12	PD, PE, ESO,AESO	Identified the condition of culverts.
14	GRC meeting	KU04 ID27	PD, PE, ESO,AESO	Discussed the problems regarding ID 27 and problem solved.
22	Meeting	Amamaduwa DS office	PD,PE,ESO,AESO	Discussed about GRC meetings.
24	Road inspection	KU02 ID 02	PE, ESO, AESO	Inspection for Environment, social and safety issues.
27	Road inspection	PU01	PE, ESO, AESO	Inspection for Environment, social and safety issues.
28	GRC meeting	PU01 ID 07	PD, PE, ESO, AESO	Discussed the problems regarding ID 07 and problem solved.
June				
04	GRC meeting	PU01 ID01	PD,PE,ESO,AESO	Discussed the problems regarding ID 01 and problem solved.
21	Road inspection	PU03 ID 17	ESO, AESO	Inspection for Environment, social and safety issues.



Fig 5.3.1: Signing Consent Letters for Land donation

5.4 CSD-SG Consultant and ESDD assistance

5.4.1 As usual during other reporting periods, CSD-Safeguard Consultant involved and assisted to resolve some issues during the reporting period January-June 2019 which had continued several weeks unresolved. Due to the Project Director's requests and ADB-SLRM requests, Consultant mediated in following social issues to resolve.

Table: 5.4.1 Summery of CSD-SG Consultants Assistances

Date	Package	Involvement	Outcome
28.02.2019	KU-03, RdId 22	Public complaint to ADB-SLRM due to delay of road construction and other construction issues	Did an investigation on complains and met complainers explained current situation, and decided to improve unsafe sections
05.03.2019	KU-03,RdId 87 KU-02,RdId 67	GRC meeting held for RdId 87 public complaint on delay of construction. Another public complaint from RdID 67 damaging tank bund by the contractor due to uses a by-road.	Explained current delay of construction of the program at RdId 87, and contractor will repair the damaged tank bund road.at RdId 67.
14.03.2019	KU-02 RdId 01	To inquire public complaint received by RDA-Chairmen	A drainage issue to be settled by the PS but voluntary land donation needed along the by-road to construct a lead away. Land owner will provide space and RDA decided to provide build-up drain alone the by-Road. Finally agreed the land owner.



Fig: 5.4.1, 28.02.219 Field Investigation on public Complaint, KU-03, RdId22



Fig: 5.4.2, 28.02.219 Field Investigation on public Complaint, KU-03, RdId22



Fig: 5.4.3, 28.02.2019 Field Investigation on public Complaint, KU-03, RdId22



Fig: 5.4.4, KU-02, RdId-01, 14.03.2019 Field Investigation to find lead away path.

5.5 Status of functions of GRM during the Reporting Period

5.5.1 GRC Formation

5.5.1.1 Within the task of development of rural roads by the iRoad program, prioritizing public participation is significant. GRC committees so far have played an important role in Project 4 and Project 5 of PIC2. The number of level 2 and 3 GRC committees which have been formed during June 2016 and active up to June 2019 are in the table below. However no formation of new GRC committees during the reporting period, 84 DS level and 236 GN level GRCs have already been formed as required considering number of rural roads to be developed.

Table: 5.5.1 GRC Committees which are being functioned

Package	No.of DS level GRCs to be formed	Already formed as end of December 2018	No.of GN level GRCs on each road to be formed	Already formed as at end of December 2018
Project 4				
Package: AP-01	06	06	17	17
Package: AP-02	06	06	12	12
Package: AP-03	04	04	12	12
Package: AP-04	06	06	19	19
Package: PO-01	02	02	18	18
Package: PO-02	03	03	26	26
Package: PO-03	02	02	11	11
Sub-Total	29	29	115	115
Project 5				
Package: KU-01	07	07	16	17
Package: KU-02	08	08	19	20
Package: KU-03	06	06	12	15
Package: KU-04	08	08	12	12
Package: KU-05	10	10	17	18
Package: PU-01	06	06	11	11
Package: PU-02	05	05	11	11
Package: PU-03	05	05	17	17
Sub-Total	55	55	115	121
Grand Total	84	84	230	236*

*Some figures included in last report have changed because more than one GRCs have been formed for some roads considering road length.



Fig: 5.5.1, Level 2 GRC Formation Meeting During 2016



Fig: 5.5.2, Level 3 GRC formation during 2016

5.5.2 GRC meetings to resolve Social Issues

5.5.2.1 Considering social and environmental impacts during construction of rural roads, level 2 (GN level) and level 3 (DS level) GRCs were formed. So far GRC system has been successfully implemented. During refreshing transect walks and GRC committee meetings, provided valuable inputs by road users to develop road designs. However during roads handing over, some GRC members shown dissatisfaction on road condition, quality, drainage system, house access etc. The limitations, such as rural road development procedures, available limited resources, construction difficulties etc. cause to provide a new road, but satisfying all the people would not be possible. Some GRC meetings held during reporting period which are included in the next section (Table: 5.5.2) are good examples. Incapable contractors' unnecessary work delays have created unresolvable issues in NWP area. There were 3 level 2 GRCs and 2 level 3 GRCs were held during the reporting period. Since two contract packages terminated, and NCP roads were completed, less number of GRC meetings were held during reporting period.



Fig: 5.5.3, At the level 2 GRC at RdId 07,PU-01 discuss issues to be clarified with the program staff



Fig: 5.5.4, 28.05.19 After having a level 2 GRC meeting at PU-01/RdId 7 field investigation on complaints



Fig: 5.5.5, 13.03.2019 Project Director explain issues at RdId 67 of KU-02



Fig: 5.5.6, CSD-SG Consultant explains iRoad program policies on resolving public issues



Fig: 5.5.7, 13.03.2019 Contractor given his explanations on damaging cause way at Rd67, KU-02

Table: 5.5.2 GRC meetings held to resolve public complaints during January-June 2019

Date	Package/Road ID	Level:2/3	Main Issues discussed	Outcome
05.03.2019	KU-03, RdId 87	2	Incomplete road construction and abundant by the contractor. Damages/potholes in the road. Insufficient road width. Safety problems.	Sections/structures partly constructed will be completed by a new contractor.
13.03.2019	KU-02,RdId 67	2	Damaging tank bund road by the contractor while using a as by-road during road diversion	Contractor will repair the tank bund.
30.03.2019	KU-04, RdId 27	3	Delay of construction by the contractor. New year festival to be held. Road to be developed before.	Contractor agreed to fill potholes for the new year festival. Permanent construction will start during May.
28.05.2019	PU-01, RdId.07	2	The issues presented at last GRC meeting still not attended the contractor. Difficulties facing by road users. Mahaweli blamed RDA canals with soil.	PD explained current situation of contractors on delay of road construction.
04.06.2019	PU-01,RDId 01	3	Damaging new road by cattle in the area.	No proper complainer and DS advised cattle farm owner

5.5.3 Summery of Progress of Public Complaints resolving up to end of June 2019

5.5.3.1 The public complain register is a basic data base of 'public participation' which received complaints, requests and suggestions from the public on the programe. The information recorded in registers from the beging of the programe (2016) and up to June 2019 are included in the following table describing number of action taken, number of rejected due to reasonable causes, number of cases which solutions are being implemented. Action taken means that the majority of them are incorporated in to road designs (3114) while some are resolved (718) not related with designs but day-to-day issues. Total number entertained up to June 2019 was 3832. *(Please see also the table No.5.5.4 for more information on the same.)*

Table: 5.5.3 Summary of Public Requests

Package	No.of Complaints/Requests/ Suggestions	Number of Action Taken	Number of rejected due to reasonable matter	Number of Cases Solution in- Progress
Project .04				
AP-01	244	229	15	00
AP-02	212	195	17	00
AP-03	346	332	14	00
AP-04	432	367	65	00
PO-01	160	150	10	00
PO-02	187	166	21	00
PO-03	119	107	12	00
Sub Total	1700	1546	154	00
Project. 05				
KU-01	424	312	112	00
KU-02	728	568	155	05
KU-03	308	289	11	08
KU-04	494	437	27	30
KU-05	233	208	04	21
PU-01	174	136	24	14
PU-02	235	212	07	16
PU-03	155	124	00	31
Sub Total	2751	2286	340	125
Grand Total	4451	3832	494	125

Summary of detailed public complaints, requests and suggestions received from the public are in the annex: 11.2

5.5.4 GRM functions from the Rural Road designing to Roads Handing over

5.5.4.1 It was continued even during the reporting period, January-June 2019, the exercise of fulfilling GRM objectives, achieving in three stages when roads handing over. First stage is preparation of the list of number of public complaints, suggestions and requests during transect walks, during GRC meetings and from public complaint register, and how many incorporated in to final designs, how many not and reasons as described. Second stage is using this list as a checklist, during road handing over visits to check how far public proposals implemented and if not, advise the contractor through the TOC (Taking Over Certificate) attachment to complete them. An example for a TOC is in the Annex: 11.5 and its purpose is reminding remaining works to do and target dates to be completed. Third stage is measuring positive social impacts which emerge.

Table: 5.5.4 Public Contribution in iRoad Program- PIC-02, Summery Table for 15 Packages, as at end of June 2019

Pack age	No.of Requests/ Complaints/Suggestions from 2016				No.of Cases – Action Taken/incor porated in to designs	No.of cases action rejected reasonably	Resolve d-Not related to designs	Solution In- progress	Total Road length (Km)
	Initial GRC meetings and Transect Walks	Public Complaints Register- During Construction	GRC meeting during Construc tion	Total Cases					
Project 4									
AP-01	31	209	04	244	173	15	56	00	85.09
AP-02	80	122	10	212	142	17	53	00	76.956
AP-03	103	241	02	346	296	14	36	00	82.71
AP-04	144	280	08	432	367	65	00	00	89.83
PO-01	102	57	01	160	73	10	77	00	50.76
PO-02	89	98	00	187	122	21	44	00	72.79
PO-03	49	70	00	119	30	12	77	00	49.41
Project 5									
KU-01	149	268	07	424	289	112	23	00	82.84
KU-02	156	558	12	728	425	155	143	05	97.36
KU-03	205	91	12	308	191	11	98	08	93.00
KU-04	238	198	58	494	423	27	14	30	105.95
KU-05	170	41	22	233	165	04	43	21	95.95
PU-01	29	139	06	174	114	24	22	14	92.73
PU-02	39	73	123	235	180	07	32	16	74.37
PU-03	85	70	00	155	124	00	00	31	48.47
Total	1,669	2,517	269	4,451	3114	494	718	125	1,198.216

6.0 Issues Observed (Social Related) and Status of Compliance

6.1 The experiences so far gained on monitoring GRM and public participation for the iRoad program is proving that the social issues are always emerged due to key main reasons.

1. Contractors' construction delays and social issues/personal issues
2. Insufficient Quality/technical status clarification; public misunderstandings
3. Lack of feedback to people on transect walks/GRC requests by PMU

6.2 During the reporting period January - June 2019, the most of social problems emerged due to construction delays, and not doing as scheduled and had promised to people. Inaugural functions of roads construction have held during 2016-2017. However most of the roads are still in the same condition no construction activities have been commenced. In some roads partly constructed sections such as ABC laid sections, asphalted sections, gravel sections, partly constructed culverts, side drains etc are remaining, long time without any construction. It was revealed at monthly progress review meetings that contractors' incapability in maintaining proper project management including keeping required human resources and financial management, ensuring required equipment and machineries, project activity planning and implementing as scheduled.

6.3 Other important factor always create social unrest is, efforts to apply engineering solutions to improve poor conditions of rural roads which were vulnerably established. All exercises from transect walks, GRC meetings and public complaints register have shown that the majority of requests are related to need of more culverts, bridges, causeways and build-up side drains to resolve water stagnation, inundation problems and access to houses. Engineering solutions are not always workable in resolving existing problems during developing existing rural roads. Unsystematically established habitats have blocked natural water flow and when improving road conditions, it affect to the existing water flow which used to divert along or crossing roads. Then the people needs more side drains and culverts but they are not willing to provide space for lead away drains. Dividing village people in to two or three groups due to the issue and when some people who are in upstream needs culvert to send water to down, cannot see any provisions for leadaways from downstream. Another common issue identified here was, although the rainwater spread everywhere vulnerably before raising the road with an asphalt layer, after construction now the people are not allowed flowing water in to their lands.

6.4 Third point could be created as a social issues is, insufficient communication between beneficiaries and developers. At the beginning, transect walks and GRC meetings close the beneficiaries to developers, however no proper feedback from developer to beneficiary how far public requests could be entertained considering technical and financial limitations until create social issues. During completion of construction of a road, or at the road handing over, public complain that they have not received what the requested during transect walks or GRC meetings. Following key social issues which could have been created 'social unrest' during reporting period would be good examples to three points mentioned above.

Table: 6.1: Social Issues and the way of Resolving during the reporting period

Road and Package	Date	Social Issue	How it was resolved
AP-03, Rd.29	09.02.19	The public expect tank bunds are also to be asphalted as other road sections. However technically it was not feasible as iRoad procedures. This could have been explained at the beginning by Engineer, Ag.Services and RDA to the public. Unnecessary public unrest created and several GRC meetings were held to resolve this issue.	Director of Agrarian services explained the same to public, and RDA promised to do macadam road for two bunds, and other to be constructed by Agrarian Services.
KU-02, Rd.01	14.02.19 25.02.19 14.03.19	Old storm water flow changed after asphaltting Rd. Id 01. Coconut estate owner opposed sending rainwater through the land. As an alternative, sending through drains of a by-road close by, was also not acceptable to by-road users. Reason was asphaltting roads and technical and physical repercussions was not known by public. Road designers could have been explained earlier.	Finally, after having 3 meetings and several field visits by all parties including PS of Alawwa, RDA decided to provide build-up drains to the by-road, land owner promised to provide a strip of land to keep by-road width after strengthening drains. Public agreed to use the by-road for sending rain water. CSD-SG Consultant was also involved since complaint has gone to ADB-SLRM.
KU-02, Rd.56	21.02.19	Public of a road section (5+800) of RdId 56, complained that no proper house access provided with Hume pipes due to two accesses were not completed. The owner of two access not agreed with the size of Hume pipes given by the contractor. Contractor has promised to purchase and provide wider Hume pipe, but not has happened. Although the road construction was over handing owner not taken place due to this problem.	The land owners requirement was not understood earlier by the contractor since heavy vehicle movement at his land he needed wider Hume pipe. SGRS and CE met both parties and expedited the solution.
KU-03, Rd.87	05.03.19	Delay of road construction as planned, difficulties to road users due to partly constructed sections of the road. Potholes create vehicle driving difficulties, water stagnation during heavy rain and dust from gravel section during dry days. Public complained to ADB-SLRM also. PD decided to have a level 2 GRC.	At the GRC PD explained the status of termination of the contractor and new contractor will be appointed. Temporary solution to cover potholes will be done. CSD-SG consultant was also involved.

KU-02, Rd.67	05.03.19	Contractor has unauthorizedly used a by-road during road diversion of Rd.67, then the damages to a tank bund and causeway occurred. As the contractor says, other heavy vehicles were also used the road. However contractor intentionally diverted traffic over the by-road, during road construction. Public complained to ADB-SLRM.	After several field visits and meetings contractor agreed to do minor repairs to the causeway and tank bund. CSD-SG Consultant was also inquired about the issue to report to ADB-SLRM.
KU-02, Rd.67	13.03.19	A Farmer Organization has complained to PD on list of problems of newly constructed road 67. PD decided to have level 2 GRC.	The complainer came late to GRC and others who participated including PS-Deputy Chairman praised the road construction. However complainer highlighted some unclear issues he found and the PD and contractor clarified them all to satisfy the participants. Due to a personal issues of villagers the problems have created and complained to RDA.
KU-04, Rd.27	22.03.19	A complaint to the Governor from a widower, Samurdhi recipient, on house cracks and the risk of falling down the roof with the electricity connection. House was in dilapidated condition however contractor did not have pre-crack survey records. Contractors' negligence of complying with construction procedures created the problem.	RE, CE and SGRS advised the contractor to negotiate with the complainer and pay a compensation. It was not possible for insurance provisions, since no pre-crack survey records. Both parties agreed an amount of money.
KU-04, Rd.27	30.03.19	Construction delay from the inaugural ceremony held during 2016. DS suggested PD to have a level 3 GRC due to continues public complaints on potholes, dust, muddy condition during raining, traveling delays due to bad condition of the road etc. Due to poor financial management of the contractor, did not have human resources, machineries, payment salaries in time, money for borrow materials etc.	PD organized preparatory meeting on 29 th for the 30 th GRC meeting. Insisted the contractor to come to the meeting with proper work plan. Meeting was held on 30 th . PD explained contractors delay due to their poor management. Contractor promised to start works from following Monday (1 st of April) and complete the road at the end of September 2019. Watering will do by the contractor.
PU-01, Rd.01	22.05.19 04.06.19	Two public complaints were on road damages due to cattle farms close to newly constructed road and when the cattle crossing road and going along road damage the road. This was a road design issue which was not considered at the beginning by all parties. Road could have had strong build up road shoulder section for the	All parties including PD, DS, GS, PE, SGRS, one complainer, GRC members were there on 4 th June. It was very strange that nobody from relevant complainers' organizations have sent the petition. Some body has used their name of the farmer organization and funeral assistance society. Then the cattle owner was advised

		crossing path of cattle. SGRS suggested to discuss the matter with the DS-Anamaduwuwa and met DS on 22 nd May by SGRS, PE, ESO. And DS suggested to call Grama Niladari, two complainers, GRC members for a GRC meeting on 4 th June 2019.	by PD to use one road section and not to take cattle along the road.
KU-02,Rd.02	24.05.19 26.06.19 (01.07.19)	Existing lead away was blocked by a resident at the road Id.02 (at 1+830), and storm water stagnation in the developed road. PS Chairmen forced the land owner to give leadaway path through his land but land owner refused. He complained that the whole front land area of their house will be eroded if a leadaway goes. PS Chairmen asked the PD to have a drain through his land but PD refused to force the land owner. Land owner finally said if it will be a build-up drain he will agree. But PD explained no sufficient funds to construct outside drains. Finally Consultant decided to concrete the road section on 27 th June, but PS Chairman opposed and stopped concreting works after placing an entry in the Police station.	Finally the issue directed to the DS-Alawwa and decided to have a level 3 GRC on 1 st July 2019. <i>(During writing this report the GRC was held on 1st July 2019 at the DS-Alawwa Office. PD explained that water stagnation in the road to be avoided by the RDA if the leadaway is not possible and the consultant decided to concrete the road section. DS asked the land owner if hume pipes provided would it be possible to have a drain through his land. Then the PD agreed to provide some Hume pipes. The problem resolved)</i>
PU-01,Rd.07	28.05.19	Public has complained during completion of the road, one and half years ago, on some construction defects in the road surface, access problems, soil erosion, road embankment frailer, road shoulder problems, broken culvert and drainage issues etc and the consultant had advised the contractor to rectify. But it was not done until 28 th May 2019. PD organized a level 2 GRC meeting to explain contractors' plans to rectify them. The same problems raised by some GRC members which raised one and half year ago. Dept. Of Irrigation was also had problems.	As usual, at the GRC meeting held on 19 th May, contractor promised to rectify all from 1 st week of July 2019. All relevant location were visited by participants and noted down after the meeting.



Fig: 6.1, 25.02.19 Resolving KU-02,Rd.01 drainage issue by PS,RDA,DS



Fig: 6.2, 28.05.19Public complaints PU-01 RdId07, None rectifying construction defects



Fig: 6.3, 28.02.19, KU-03, Rd 22, Resolving public unrest on delay of completion of road construction



Fig: 6.4, 05.03.2019, KU-03, Rd 87 Resolving public unrest on delay of completion of road construction

6.1 SGRS's Observations on Social Issues so far emerged in iRoad Program (PIC2)

6.1.1 The participatory development approach including GRM strategies help to implement rural road development programs, in consideration with social and environmental complexities in rural areas. The most of the rural roads of the i Road program are basically emerged from foot paths, cart tracks, which were the ancient people used to go to Chena, paddy field, forest, closest village or a town far away. In short, the most of these roads were not developed using engineering knowledge. During populating, human habitats were established in an unplanned manner without considering geographical features or landscape in the area. However, when these roads needed improvements itself due to the spreading resources from main cities in to these remote areas. Carrying out IEEs and SIAs, surveys for engineering designs were must to grasp socio-economical and engineering requirements to formulate a rural roads development programs.

6.1.2 Now the iRoad program is being implemented in Southern, Sabaragamuwa, Central, NCP, NWP and Kalutara District. As the experiences of PIC 2 social safeguard monitoring during last four year period, following points are highlighted by SGRS of PIC2 as lessons learnt. These points and could be used for formulating future programs.

1. Criteria of categorization of rural, urban and semi urban roads to be clarified considering socio-economic and geographical condition in different DS Divisions. As an example, 'rural roads' are deferent to each other when considering roads in Kurunegala district and in Polonnaruwa/Anuradhapura/Puttalam colony schemes. While the space to widening the road is available in Polonnaruwa, some of roads in Kurunegala have the maximum of 2.5 m. It is unavoidable that the people in all four districts request to widen the roads, narrow culverts and bridges etc. Increasing socio- economic activities close to roads in some areas, with more flat land area would not be ignore similer requests. *It is suggested to have a flexible methodology in deciding road width in rural roads, considering socio-economic conditions and available land area.*
2. Due to unplanned vulnerable development of most of the rural roads, all public requests cannot be entertained by the program. Reducing water stagnation or inundation cannot be resolved without having sufficient number of culverts with lead away drains. In most area public reluctant to provide space for lead away drains or they have encroached potential locations. It is a huge challenge to the Design Engineers of the program to provide proper and sustainable drainage system in the area. It is also unavoidable, continuation of public suffering due to inundation problem which is also increased after raising the road. *Drainage Experts inputs needed during doing EIA/SIA or/ and during road designing. Drainage plan to be explained at the planning stage to the people if it is necessary.*
3. There are public requests as well as engineering and social requirements of widening and strengthening very narrow and old culverts, bridges, causeways during asphaltting roads. Since the request directly link with the road safety. However, it seemed that no provisions or sufficient funds available within the program to respond public requests to comply with GRM policies. *During transect walks and road planning stage, it is necessary to build an awarnes among the people on possible construction methods, technical limitations and available resources for the development of the road.*
4. At the beginning the program, it commences with very democratic way following public participatory approach such as transect walks, establishing village level and DS level GRCs etc. All these activities support developers to grasp public requirements for the road development as planning and implementation inputs. However, no proper and accepted way to communicate back to the people by developers, on what can be carried out and what cannot be through the programe and reasons caused prior to construction. Program monitors found that in some roads until completion the road, people are not aware what they receive or not. Then they do protests or campaigns against rest of the program activities. Also some issues still remained unresolved or took long time to resolve, which could have been resolved through proper communication at the beginning. Tank bund development,

culverts and lead away issues, house access, build-up drains, widening bends or narrow sections etc. are better examples. It is also questionable that how far the social safeguard policies including GRM could serve the programme in smooth implementation. *It is recommended that proper two way communication system to be established during the planning stage of each road construction.*

5. Except few issues which are still being resolved, the most of the issues were resolved using GRM and using public participatory approach in PIC2 program. The challenge still remains here is even very few unresolved issues in some roads, affects badly to construction time schedules. Employer, Contractor and Consultant spend more time with the people and Government officers to resolve them. *However, 'two-way communication' method to be used prior to implementation of road development activities in consideration with incorporating public contribution in to rural road development.* Beneficiaries should know in advance the reasons for which requests will be incorporated in to designs and why their some requests are rejected.
6. *Proper assessment of the capacity of contractors for selection specially for the i Road program should be an important, inconsideration with development policies such as public participation and social safeguard requirements.* The majority who incapable contractors destroy the iRoad project core which is targeted humanistic development objectives. Contractors are selected by the Government Authorities and handed over to Project Directors of PIUs and Team Leaders of Consultants. The most of these contractors' management weaknesses are come out only during program implementation. It is observed that the inability to implement the programme as planned and scheduled by the contractor is because of their insufficient human and physical resources. The majority of contractors are reluctant to follow existing Sri Lankan labour laws and to comply with Environment Act and ADB guidelines. Although there are contractual procedures to control contractors by PDs of PIU or TL of consultant, such actions will also affect to the programme progress. Contractor directly and indirectly create more social and environmental issues in the field without completing roads as planned. Partly constructed roads, culverts, drains etc are always risks of accidents to the road users. *Hence the social safeguard principles and practices are becoming invalid and contradictory. The negative social impacts which are emerged due to these causes which are always beyond the control of social safeguard team of the programme.*

6.1.3 Despite the implementation difficulties of social safeguard activities and the GRM as described above, the majority of roads in project 4 were developed successfully using public participation including in NCP and KU-01 in NWP. Completion of roads as scheduled and in better quality, reduce public complaints and protests against RDA.

7. Progress in Land Donation

7.1 Appendix 3 of Resettlement Framework describes basic principles and procedures of land donation enable project team to comply with social safeguard requirements. As the TOR of SGRS states 'for rural roads require land donation, monitor the land donation process and conduct field visits on a monthly basis until land donation is finalized'. Since the land acquisition or resettlement of people are not with implementation procedures of the iRoad program, programme is under the category C, and it has to be carefully monitored, the people who willing to donate their land strips as a result of requirements exists to improve the safety/road condition at some road sections. This exercise is completely depending on the positive public consultations with land owners.

7.2 This section briefs how the public consultation process carrying out by PIU, PIC2 and the Contractor while complying with social safeguard requirements during the reporting period. As states the TOR of the PIC2, SGRS to monitor the land donation process and join with PIU Social Safeguard team and ESO of contractor when they having consultations and receiving consent letters from land donors. PIC2 should ensure that receiving additional land portions are technically and socially feasible and land owner her/himself decides to give land free and without any compensation.

7.3 The request come from the contractor and then the Resident Engineer (RE) to organize joint investigation, to finalize whether additional land needed or not. If needed it will be informed to PIU by the RE. Then the Contractor to peg-out required land strips to be taken and negotiation with land owners will be carried out by SSO/ESO of PIU with Project Engineer and with SGRS if necessary. Then receive consent letters from land owners stating they are voluntarily provide relevant land strip.

Table: 7.1 **Summary of completing ‘Consent Letters’ as at end of June 2019**

	AP-01	AP-02	AP-03	AP-04	PO-01	PO-02	PO-03	KU-01	KU-02	KU-03	KU-04	KU-05	PU-01	PU-02	PU-03
January-June 2019	-	-	01	-	-	06	-	-	-	-	02	-	-	-	05
Cumulative	12	09	44	14	52	77	01	110	343	82	24	41	24	30	12
Total	Project:04 = 209			Project: 05 = 666			Total: 875								



Fig: 7.1, Signing Consent Letters by ESO-PIU with the land owner, NWP



Fig: 7.2, Signing Consent Letters by ESO-PIU with the land owner –NWP



Fig: 7.3, Signing Consent Letters in NCP



Fig: 7.4, Signing Consent Letters in NCP

8. Coordination with Line Agencies and Sharing Experiences with other PICs

8.1 Since basically the rural roads are not belonged to RDA, during implementation of iRoad program it was necessary to coordinate with other agencies such as Local Authorities, DS Offices, PRDA, Telecom, CEB, Dept of Irrigation, Mahaweli, Dept. of Forest, CEA, GSMB and the political authority. Main purpose of having communication with these agencies is for the smooth implementation of the i Road program as scheduled and to achieve planned results. The PIU-RDA in each province take this coordination responsibility and so far continuing successfully.

Table 8.1

NCP – Recorded information on coordination with line agencies during January-June 2019

Date Location	Purpose	Institution	Institution (participant)
2 nd February	Divisional Coordinating meeting (Progress review meeting)	DS Office Rambewa	DS & All Relevant government staff
12 th February	Divisional Coordinating meeting (Progress review meeting)	DS Office Padaviya	DS & All Relevant government staff
February	Divisional Coordinating meeting (Progress review meeting)	DS Office Palagala	DS & All Relevant government staff
12 th February	Divisional Coordinating meeting (Progress review meeting)	DS Office Palugaswewa	DS & All relevant government staff
28 th February	Divisional Coordinating meeting (Progress review meeting)	DS Office Kakirawa	DS & All relevant government staff
March	Divisional Coordinating meeting (Progress review meeting)	DS Office Rabewa	DS & All relevant government staff
18 th March	Divisional Coordinating meeting (Progress review meeting)	DS Office Padaviya	DS & All relevant government staff
08 th April	Divisional Coordinating meeting (Progress review meeting)	DS Office Rabewa	DS & All relevant government staff
09 th April	Divisional Coordinating meeting (Progress review meeting)	DS Office Padaviya	DS & All relevant government staff
17 th May	Divisional Coordinating meeting (Progress review meeting)	DS Office Rabewa	DS & All relevant government staff

31 st May	District coordinating committee meeting (To discuss issues & progress)	District secretariat office - Polonnaruwa	MPS & All government staff
17 th June	Divisional Coordinating meeting (Progress review meeting)	DS Office	DS & All relevant government staff
27 th June	District coordinating committee meeting (To discuss issues & progress)	District secretariat office - Anuradhapura	President / MPS & All government staff

Table: 8.2

NWP-PIU records of Meetings participated during reporting period			
	Date	Package	Divisional Secretary Office
1	11.02.2019	KU5	Polpitigama
2	11.02.2019	KU4	Ambanpola
3	21.02.2019	KU3	Kurunegala
4	04.03.2019	KU1	Kuliyapitiya (West)
5	11.03.2019	KU1, KU5	Panduwasnuwara -East
6	29.04.2019	KU5	Nikaweratiya
7	29.04.2019	KU4	Kotawehera
9	03.05.2019	KU5	Kobeigane
Road safety meeting			
1	29.03.2019	KU4	Police Station- Nikaweratiya
2	26.04.2019	KU4	Police Station- Nikaweratiya
3	30.04.2019	PU1	Divisional Secertarty Office- Karuwalagaswewa
District Coordinating Committee Meeting			
1	24.06.2019	Puttalam	

9. Public Consultations and Information Dissemination

9.1 It is necessary to understand by any development program manager, that the public should know what is happening under the labels of 'Development Projects'. It is believed that public consultation and information dissemination should be the first step of any development program. The iRoad development proposal must have assessed by the ADB and they have considered that the development of existing rural roads in Sri Lanka important for linking the rural hub in to urban resource center. However the carrying out the task of development of these roads to be carryout carefully considering the sensitivity of rural environment. Therefore the Resettlement framework and the Facility Administration Manual (FAM) has included guide lines for how to do planning and implementation public participation in to the program. The Grievance Redress Mechanism (GRM) was recommended and formation of three levels of GRCs emphasized proper consultation of relevant parties including road users in road development. The whole i Road program planning/designing, implementation and operation stages are guided by the concept of Context Sensitive Designs (CSD), and the beneficiary participation is become as an important tool in this task.

9.2 As the TOR of SGRS states 'monitor and evaluate implementation of the community participation process prescribed in the RF and recommend improvements to the participatory process to ensure broader participation of rural population in project roads formulation'. Since, public consultation and information dissemination during project implementation has to be played a significant role in the field. This section of the report consider the activities and measures have been taken by PIU, PIC2 and Contractor related to public consultation and information dissemination in the project 4 and 5 area specially during the reporting period from January to June 2019.

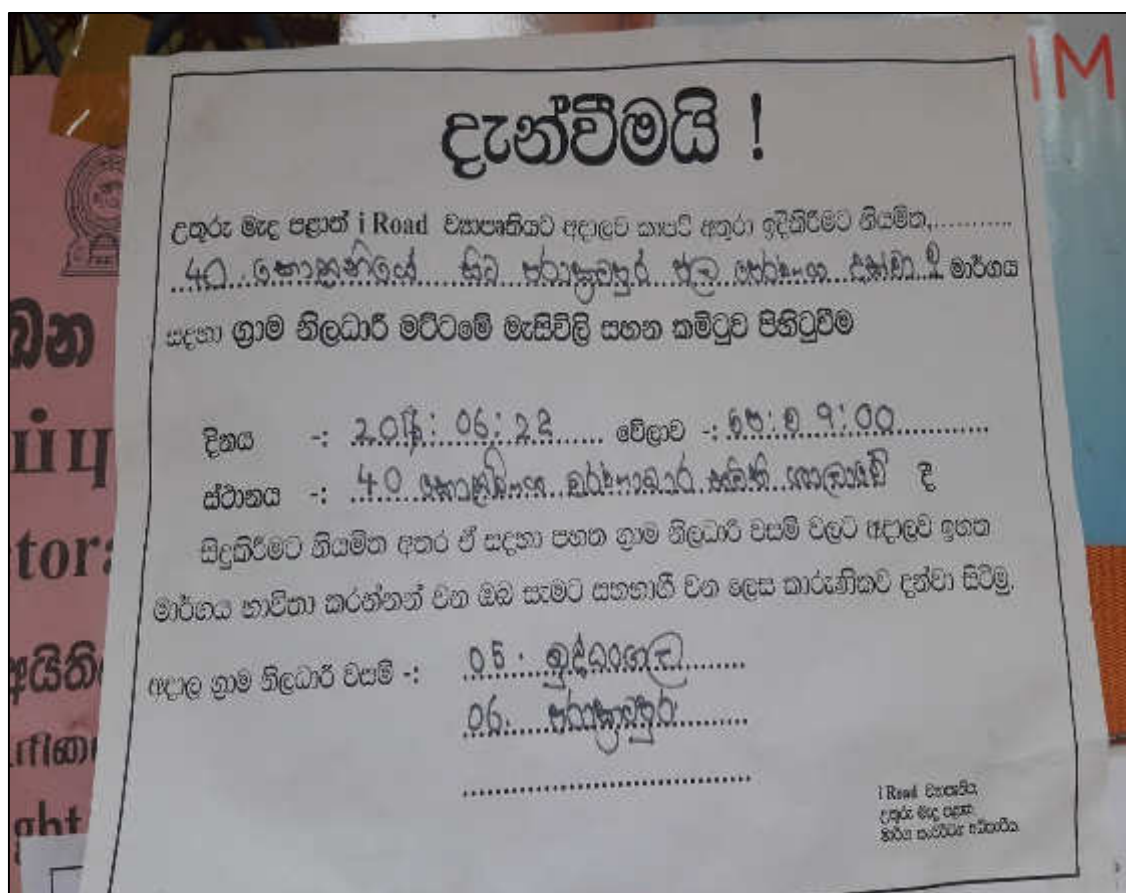


Fig: 9.1, First communication for information dissemination is formation of GRCs.



Fig: 9.2, GN Level GRC formation convey the iRoad Message to the village (during 2014-2016)



Fig: 9.3, Information on how to develop their roads exchanged between two parties



Fig: 9.4, Basic Information on the road development publicizes in the sine board



Fig: 9.5, Publicizes contact persons, and steps of activities through posters.

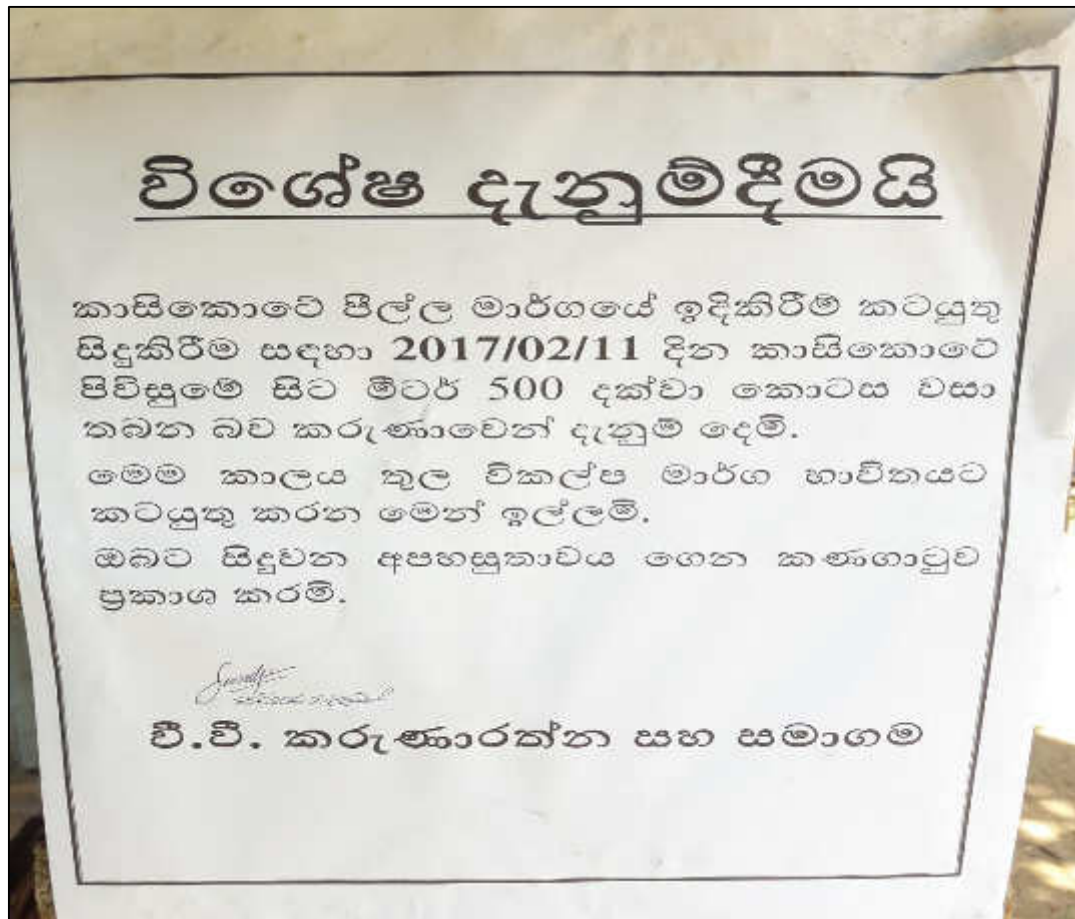


Fig: 9.6, Temporary traffic diversions may expect during road construction and public are informed



Fig: 9.7, Contractor is responsible to disseminate all relevant construction information



Fig: 9.8, Complaint boxes and public notices with contact person information are in public places



Fig: 9.9, Contractor is responsible to disseminate all relevant construction information



Fig:9.10, Complaint boxes and public notices with contact person information are in public places



Fig: 9.11, Contractor prioritizes road safety while communicating with road users



Fig: 9.12, communicating village people in typical way by some contractors (PO-01, MÄGA)



Fig: 9.13, communicating village people in typical way by some contractors (PO-01, MÄGA)

9.2 As above photographs described during program planning and during program implementation, communication and dissemination of program information between public and developers were ensured. However public consultation and information dissemination are not limited only in to construction activities of the iRoad program. iRoad road safety communication provide guidance for the construction sites considering safety of people moving there and on the other hand, road safety know-how to the public who will use developed roads. Awareness on how to manage in new carpeted roads minimizing road accidents was also emphasized in road safety programmes. The communication and public consultation programs of iRoad, spread over public health programs considering human development without limiting physical development. It was explained in previous sections of this report, the public road safety programs and public health programs have implemented during reporting period.



Fig 9.14 These leaflets were circulated among public during road safety public programs. Section 4.2 of this report describe the iRoad road safety public program.

9.3 Although the above measures on public consultation and program information dissemination was ensured and implemented by some contractors, a considerable gap between the beneficiaries and implementers are observed during the reporting period from January-June 2019. It is suggested that there are two kind of program implementation practices have caused to create this gap.

1. **Negligence on road safety methods:** In some contract packages, road users were not properly informed in advance on some major construction activities and or on road diversion locations. Road closing and diversion have happened in several places without knowing the public. Poor safety measures in construction sites create unnecessary hazards to the public and public relations with the contractor has damaged.
2. **Lack of required capacity and poor program management:** Poor work progress of some contractors have created great difficulties to the public. KU-03, KU-04, KU-05, PU-02, PU-03 in NWP are having abundant and partly constructed roads. Some construction sites are still remaining, long time barricaded or without proper safety measures creating difficulties to road users. These contractors were not able to follow Consultant's instruction as required since they were having financial shortages within their management.
3. However the GRM of the program surves to minimise pulic unrest incidents, when organising GRC committee meetings by the PIU and explaining reasons of social issues public suffer, is still appropriate to reduce the gap between benefitiories and implementers. There were 10 GRC meetings were held during reporting period to clarify public issues.

10.0 Conclusions and Recommendations

10.1 Conclusions

10.1.1 The reporting period from January-June 2019 is providing satisfactory programme implementation results from Project-04, NCP, while less satisfactory progress from Project-05, NWP. Road construction activities of NCP and KU-01 of NWP were almost completed during the period. All three contract packages in Polonnaruwa district completed during December 2018 and January 2019. Resolving actions on social safeguard issues related to house accesses, drainage systems and leadaways in NCP which have been emerged during last two reporting periods, successfully carried out by Contractors and they were able to complete their targets satisfactorily during the reporting period. Also the contractors in NCP and KU-01 in NWP implemented and completed road safety public awareness programs as planned with school children, parents, three wheel drivers and motocyclists during the reporting period.

10.1.2 However, it is questionable whether the public participatory approach and context sensitive designing objectives achieved during the reporting period. Termination of two contract packages caused negative social/environmental impacts directly and indirectly. In other contract packages in NWP, such as KU-04, PU-02 and PU-03, continuously reported unsatisfactory construction progress during the reporting period. At the same time delay of construction created partly constructed unsafe road sections, culverts, drains etc with accident risks to the public. However the PIU and Consultant were able to take remedial measures to reduce unsafe sections in KU-3 and KU-5. Some public protests were also there which managed using GRC meetings by PIU and PIC. SGRS, RE-NWP and PIU staff always visiting and highlighting these public issues complying with social safeguard guidelines and providing instructions to contractors. Contractors' internal management problems were beyond the control of social safeguard program team, such as lack of human and financial resources within them affected badly to ignore social safeguard requirements and it caused to create unrest situation in the field staff and road users in NWP.

10.1.3 Concepts of Social safeguard and Public participation through iRoad Program are being challenged. If the situation continues with these challenges further, which are beyond the control of safeguard team of the programme, to be considered remedial actions on changes of iRoad programme strategies. However the GRM and GRC mediation during the reporting period were shown satisfactory results so far in communicating and resolving these social issues have borne within the beneficiaries.

10.2 Recommendations

10.2.1 Going back to the programme conceptualising and planning stages in the project cycle is advisable and having a program review with social safeguard PIC teams is necessary to organize by RDA management and use 'lessons learnt' for future iRoad programs. Section 6 and 6.1 in this report includes some comments with examples on SGRS's observations which are opened to discuss during programme reviewing.

10.2.2 Criteria of categorization of rural, urban and semi urban roads to be decided considering rapid changing nature of socio-economic and geographical conditions in different DS Divisions. Typical rural roads construction procedures of iRoad programme should have a flexibility in changing of road width appropriately and widening and construction of structures such as culverts, causeways, build up drains, small bridges, tank bunds etc.

10.2.3 It has to be ensured that the selected contractors for iRoad programme are necessarily aware on social/environmental safeguard principles and their commitment on why those measures to be implemented by them during construction. Proper and compulsory assessment on financial, human and physical resources and contract management capacity of contractors to be carried out by RDA before selecting them for iRoad program. It was proved during the reporting period, that the majority of contractors who shown lack of capacity within them, on those programme management requirements, destroy the 'core' of iRoad programme objectives which are targeted human friendly development goals.

11.0 Annexures

11.1 Project Map

11.2 Summery of Public Contribution

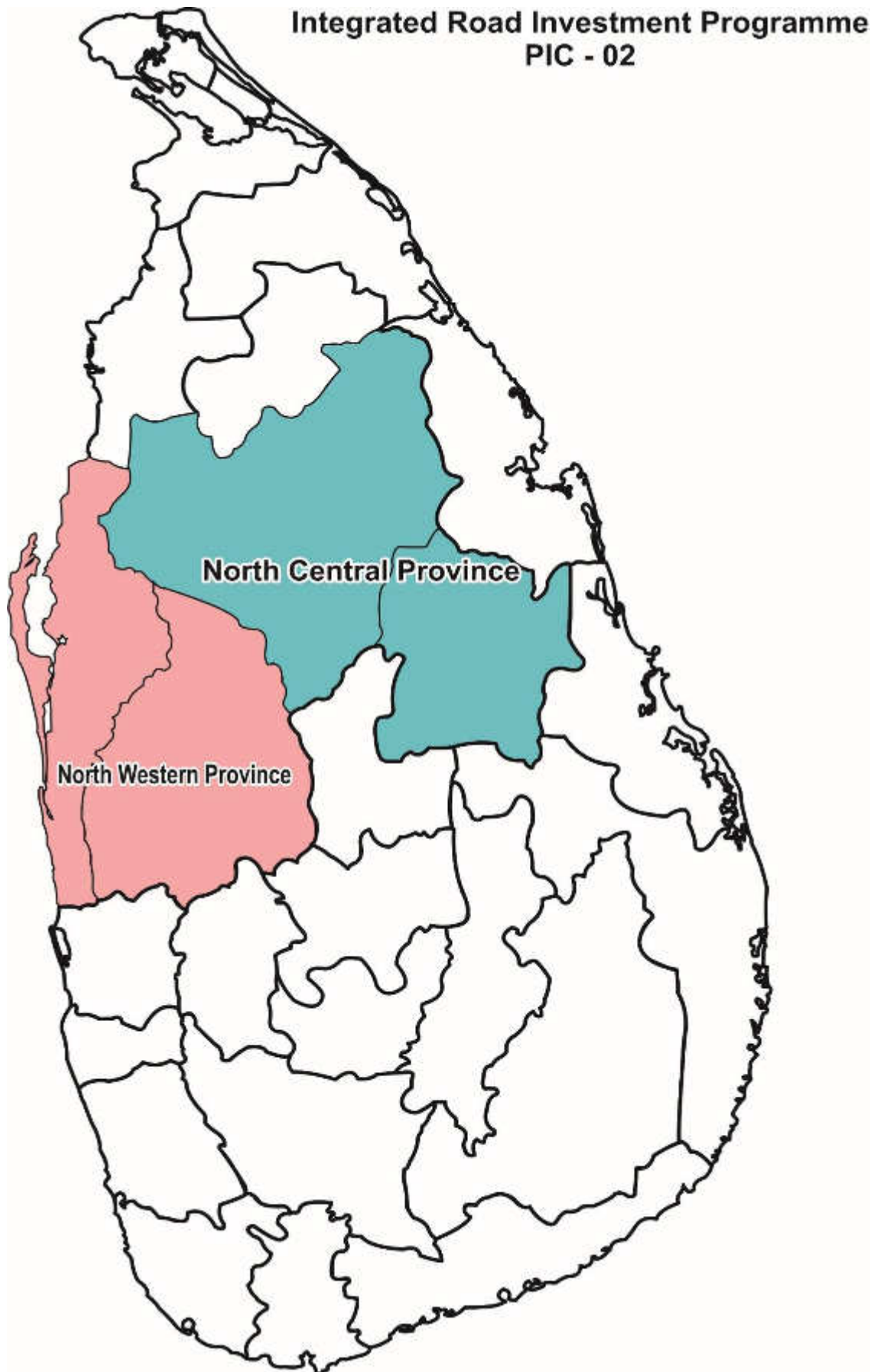
11.3 Construction Development

11.4 Case Study

11.5 Specimen of Taking over Certificate (TOC)

11.6 Land Donation Registry

Annex: 11.1



Annex: 11.2

Information summery of Transect Walks, GRC and Complaint registry (2016 to 2019 NCP, NWP)

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
Project 4 - Anuradhapura (NCP)					
AP 01	244	229	15	00	<ul style="list-style-type: none"> ▪ The requests are house or land accesses more than one or two for each. But according to the project scope and BOQs cannot give multiple accesses without reasonable matter. ▪ The request is, soil which are used for by-roads for compact and landing the Paddies. But according to the Agrarian Law strictly prohibited to landing the Paddies. ▪ The requests are new culverts. But according to the engineering observations they are not relevant in those places and also side drains solve the water runoff or flooding issues. ▪ The request is to do shoulder constructions while asphaltting. But in technically have to do shoulder constructions after the asphalt paving.
AP 02	212	195	17	00	<ul style="list-style-type: none"> ▪ The request is to construct a new culvert. But only repaired and cleaned the existing culvert without constructing a new culvert. ▪ The two requests are to increase the culvert size. But only provide earth drains and cleaned the culverts due to lack of fund. ▪ The request is to construct new culverts, but due to lack of provisions only provide earth drains and cleaned the culverts. ▪ The request is to construct damaged lake dam, but it is forwarded to Department of Irrigation.

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
					<ul style="list-style-type: none"> ▪ The request is to construct new culverts, but only increased the sizes of identified some existing culverts. ▪ The request is to construct concrete drainage structures along the road especially near the paddy fields. But due to lack of funds it cannot be done. Hence, proper earth drainage facilities will be introduced. ▪ The request is to construct new box culvert in large size instead of existing present culverts. But PIU is not accepted it. ▪ The request is to lay the pipe lines cross the road. But it is discussed and banned the illegal pipe line crossing. ▪ The request is to construct a bridge over spill. But due to lack of funds, it cannot be accepted. ▪ The request is to widen the Match - box Bridge. But due to lack of funds, it cannot be accepted. ▪ The request is to renew Irrigation structures in parallel to the road. But it cannot implement in this project, so that forwarded into the Department of Irrigation. ▪ The request is to renew all the culverts across the road. But only two culverts will be renewed. ▪ The request is to renew the bathing places including steps also. But access roads will be provided to those places. ▪ The request is to renew the irrigation structure due to leakage in it. But it cannot do in this project due to lack of funds. So that it is forwarded to Department of Irrigation. ▪ The request is to concrete a road section. But it cannot implement due to lack of funds. ▪ The request is to construct a side drain on Lake Dam side. But already earth drain will be facilitated.

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
AP 03	346	332	14	00	<ul style="list-style-type: none"> ▪ The request is to construct a new culvert. Actually decided to reconstruction. But when after constructed a proper leader way and an earth drain, technically confined there is no need a new culvert. ▪ The request is to construct a new culvert. But the land owner disagreed to release his own land for leader way. Therefore, it was not accepted. ▪ The request is to increase the culvert capacity. Then cleaned the leader way and earth drain. Therefore, no need to capacity improvement. ▪ The request is to construct a new culvert. But it is no need a new culvert because earth drain is suitable for proper water runoff. ▪ The request is to construct a new culvert because water flows over the road surface and stagnating. So raised the road level and formed the earth drains. ▪ The request is to construct a new bridge. But in this project there is no bridge constructions. So that it was rejected by PIU. ▪ The request is to construct a Hume pipe. But only can get action after handed over to the PRDA. ▪ The request is to construct a new culvert. But the requested people mentioned location is not given permission for the lead away by the land owner. So it is rejected. ▪ The request is to construct a new culvert. But according to the technical aspects there is no need a culvert for that location. ▪ The requests are house or land accesses more than one or two for each. But according to the project scope and BOQs cannot give multiple accesses without reasonable matter.

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
AP 04	432	367	65	00	<ul style="list-style-type: none"> ▪ The request is to construct a new culvert. But villagers said that they do not a culvert for that location. ▪ The request is to carpet the Tank bund also. But it has given to the relevant party to get the permission. ▪ The request/suggestion is to increase the culvert capacity. But the land owner doesn't like to give his land for leader way. ▪ The request is to construct an additional opening in the culvert because water stagnating due to insufficient opening in the existing culvert. But the downstream people don't like it. ▪ The request is to increase the capacity of the existing culvert. So the lead away cleaned and formed the earth drain. Hence no need capacity improvement. ▪ The request is to remove the Koon tree. But it is not effect for the visibility of the road. So no need to remove it. ▪ The request is to construct a new culvert. So the lead away cleaned and formed the earth drain. Therefore no need a new culvert. ▪ The request is to repair the course way. But technically decided that it is no need. ▪ The request is to improve the capacity of the culvert. But it is reconstructed and made a proper leader away. So that no need to improve the capacity. ▪ The request is to improve the capacity of the culvert. But it is reconstructed and made a proper leader away. So that no need to improve the capacity. ▪ The request is to increase the capacity of the existing culvert. So the lead away cleaned and formed the earth drain. Hence no need capacity improvement. ▪ The request is to construct a new Box culvert. But cleaned the leader away and formed the earth drain. Then no need a Box culvert.

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
					<ul style="list-style-type: none"> ▪ The request is to construct a new culvert at the end of the road outlet. But technically it is not accepted. ▪ The request is to increase the capacity of the existing culvert. So the lead away cleaned and formed the earth drain. Hence no need capacity improvement. ▪ The request is to construct a new culvert because water flows over the road surface and stagnated. But the lead away cleaned and formed the earth drain. Hence no need capacity improvement. ▪ The request is to increase the capacity of the existing bridge. So after the joint inspection it is accepted to establish a new Box culvert. ▪ The requests are house or land accesses more than one or two for each. But according to the project scope and BOQs cannot give multiple accesses without reasonable matter.
Project 4 – Polonnaruwa (NCP)					
PO 01	160	150	10	00	<ul style="list-style-type: none"> ▪ The request is to construct a new bridges on course way locations. But BOQ quantities are limited and therefore it is not accepted. Also Bridge construction is out of the project scope. ▪ The request is to construct an earth drain system in the area which is not belong to the project. (Section where the road has been already constructed under a separate project). Hence it cannot accept because BOQ quantities are limited. ▪ The request is to reconstruct a culvert with a higher discharge capacity, in order to eliminate the following storm water across the road during rainy periods. So as per the observation, existing one was a course way bridge, and there was no provisions and also a necessity to replace it with a fly – over bridge. ▪ The request is to increase the discharge capacity of the existing

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
					<p>culvert, in order to prevent water over flowing during rainy season. But it was identified that the existing culvert is having enough discharge capacity, hence no need to reconstruct it, but having necessary repairs to the culvert.</p> <ul style="list-style-type: none"> ▪ The request is to reconstruct a new culvert or a new bridge at the locations where there is a course way is located. But the BOQ quantities are limited and so it is not possible to construct a bridge. ▪ The request is to remove the existing culvert and divert the water flow into the next culvert. But when considering the water flow, it's unable to remove the existing culvert and the distance to next culvert is 200m. So it is hard to implement it. ▪ The request is to construct a culvert on an access road to control the risk of a local flood. But no need a culvert to accommodate the water flow, instead a properly maintained earth drain system would suffice. ▪ The request is to construct a new culvert to control the massive water flow from the road. But no need a culvert to accommodate the water flow, instead a property maintained earth drain system would suffice. ▪ The request is to Increase the road width up to 4m. In this project there are no widening of roads. Only rehabilitate the existing width up to 3m. So that according to the BOQ quantities it cannot be accepted. ▪ The request is to Increase the road width up to 5m and shoulder width 1m either side. In this project there are no widening of roads. Only rehabilitate the existing width up to 3m. So that according to the BOQ quantities it cannot be accepted. ▪ The request is to include a round er bout at the 3-way junction. But the available space is not adequate for a roundabout. But it was decided to do

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
					road surface marking as a roundabout.
PO 02	187	166	21	00	<ul style="list-style-type: none"> ▪ The request is to widen the road width minimum up to 4m. But in this project only existing width is rehabilitating. ▪ The request is to construct a new culvert. But the BOQ quantities are not enough to do it. ▪ The request is to widen the road width minimum up to 4m. But in this project only existing width is rehabilitating. ▪ The request is to implement proper drainage system at junction. But the BOQ quantities are not enough to do it. ▪ The request is to construct a new culvert. But the BOQ quantities are not enough to do it. ▪ The request is to construct a Box culvert. But the BOQ quantities are not enough to do it. ▪ The request is to construct the bend location properly visually ahead. But cannot be applied in this project. ▪ The request is to widen the road width minimum up to 4m. But in this project only existing width is rehabilitating. ▪ The request is to widen the bridge otherwise it is not enough for movement of vehicles. ▪ The request is to construct a new culvert otherwise water flows across the road in this area. But the BOQ quantities are not enough to do it. ▪ The request is to provide removing interlock for by roads. It is handed over to Pradeshiya Sabha. ▪ The request is to widen the road width minimum up to 4m. But in this project only existing width is rehabilitating. ▪ The request is to provide removing interlock for by roads. It is handed over to Pradeshiya Sabha. ▪ The request is to widen the road width minimum up to 4m. But in this project only existing width is rehabilitating.

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
					<ul style="list-style-type: none"> ▪ The request is to construct a new culverts. But the BOQ quantities are not enough to do it. ▪ The request is to widen the road width minimum up to 4m. But in this project only existing width is rehabilitating. ▪ The request is to expand the culvert width and height to prevent any flood. But the BOQ quantities are not enough to do it.
PO 03	119	107	12	00	<ul style="list-style-type: none"> ▪ The request is to reconstruct the existing culvert. But because of the lack of the provision it cannot be accepted. ▪ The request is to construct the drains on the both sides of the road. But the BOQ quantities are not enough to do it. ▪ The request is to widen the road width minimum up to 4m. But in this project only existing width is rehabilitating. ▪ The request is to construct a Box culvert at the starting point. But the BOQ quantities are not enough to do it. ▪ The request is to changed design from culvert to bridge. But the BOQ quantities are not enough to do it. ▪ The request is to shift the culvert in to the leader way location. But the BOQ quantities are not enough to do it. And also technically it is not needed. ▪ The request is to construct the drains on the both sides of the road. But the BOQ quantities are not enough to do it. ▪ The request/suggestion is to construct a new culvert. But the BOQ quantities are not enough to do it.
Project 5 – Kurunegala (NWP)					
KU 01	424	312	112	00	<ul style="list-style-type: none"> ▪ The requests are to asphalt the entrance of shops/Medical Centers/any other government or private offices. But according to the project there is no allocation for those. They are out of the project scope.

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
					<ul style="list-style-type: none"> ▪ The requests are house or land accesses more than one or two for each. But according to the project scope and BOQs cannot give multiple accesses without reasonable matter. ▪ The requests are to change the culvert positions to avoid drainage issues. But existing culvert positions cannot change, therefore solutions implemented for the drainage issues relevantly. ▪ The requests are to provide accesses with the Hume pipes. But those areas have earth drains, therefore it was not needed Hume pipes. ▪ The complaint is about crack formation due to road works and asking compensation. But according to Pre-crack surveys they were happened before the construction have been started.
KU 02	728	568	155	05	<ul style="list-style-type: none"> ▪ The request is to Construction of side drain along the road. But it is out of the scope. ▪ The request is to construct Pipe crossings for water lines. But no allocation for those. ▪ The request is to improve the Road which is connected to Katupotha. But no allocation for the extra works. ▪ The request is to widen the road width. But in this project no widen is allowed. ▪ The request/suggestion is to take existing paved interlock blocks for the temple. But those are belong to Pradeshiya Sabha and it was given to them. ▪ The requests are to asphalt the entrance of shops/Medical Centers/any other government or private offices. But according to the project there is no allocation for those. They are out of the project scope. ▪ The requests are to increase the width of the accesses. But according to the project scope no allowing widening. Therefore such requests cannot complied.

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
					<ul style="list-style-type: none"> ▪ The requests are to provide new accesses. But their existing accesses are not damaged by road rehabilitation. Therefore, they were rejected reasonably. ▪ The requests are to asphalt their access roads. But according to the project scope there is no allocation/BOQs for those. Completely out of the project objectives. ▪ The requests are to provide Disposal Soil for the houses which are not in the iRoads. Therefore, no authority to give Disposal soil for them. ▪ The Complaint as the water in adjacent wells have dropped due to dragging water from the newly dragged well at the land of Mr. E.M. Laxman Sri Rajawansha. But that was not agreed because another well in the same land did not show water deference due to dragging. Though, water consumption was stopped at 1 time due to another source was found nearby. ▪ The request is to build Line drain. But according to the Engineer's technical solutions already it was constructed a hard drain and a Kerb drain. ▪ The request is to construct a retaining wall/Protecting wall for the near Paddy areas and other sloppy areas. But already wing wall/other required protective constructions were done according to the Engineer's advice. ▪ The complaints are about crack formations due to road works and asking compensation. But according to Pre-crack surveys they were happened before the construction have been started. ▪ The request is access to Paddy field over canal with retaining wall. But it was out of the project scope. ▪ The requests are for implement U-drains for the accesses. But according to the site conditions, the Engineer and the other parties concluded to provide Hume pipes. Otherwise implementing U-drains is time consuming and need more allocation.

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
					Therefore difficult to maintain the budget of the project when providing the U-Drains.
KU 03	308	289	11	08	<ul style="list-style-type: none"> ▪ The request is to construct the Irrigation channel, but Irrigation channels are not constructing by our project. ▪ The request is to widen the road by taking addition land strip, nut no any land acquisition in this project. ▪ The request is to rehabilitate the two bridges in this road, but bridges are not constructing by in this project. ▪ The request is to widen the road width, but road can't be widened up to 4m because not enough of the available funds as well as no widen take place in this project. ▪ The request is to construct drains along the road side 0+000 to end, but it is impossible due to limited funds.
KU 04	494	437	27	30	<ul style="list-style-type: none"> ▪ The request is that Land acquisition has been required from Kasikote Temple side. But it is not required. ▪ The request is to provide house accesses. But in this project only provide which are removed while construction purposes. No other accesses are providing. ▪ The request is the road lengthen up to the Meeoya Bridge. But BOQ quantities are not sufficient for it. ▪ The request is to widen the road width. But it is not widen the roads width in this project. Only existing width is concerning. ▪ The request is to widen the Lake bund area. But there is no sufficient funds for implement it. ▪ The request is to need to widen within the bund area during rainy season, otherwise flooding the paddy field near Nlumpathwewa Lake. But BOQ quantities are not sufficient. ▪ The request/suggestion is to mark the demarcation of the road reservation area. But there is no authority to do it from the RDA.

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
KU 05	233	208	04	21	<ul style="list-style-type: none"> ▪ The request is to elevate the Tank Bund, but cannot be widened or elevated because it is a property of Pradeshiya Sabha. ▪ The request is to remove the clock tower located at the end point of the road. But amount of land along the road is limited. So it needs donated land strip for the implement the request. ▪ The request is to reconstruct the damaged culvert, but there is no any culvert at that location. ▪ The request is to reconstruct the insufficient culvert openings, but there are no any culverts at those locations, so that willing to construct retain walls.
Project 5 – Puttalam (NWP)					
PU 01	174	136	24	14	<ul style="list-style-type: none"> ▪ The request is to build a concrete drain at flood areas. But recommended for only suitable places. Not each and every flooding areas. There is not an allocation fund for that. ▪ The request is to remove the boundary wall at the sharp bend. The general permanent structures will not be removed. ▪ The request is for construct a bridge instead of a coarse way. But money is only allocated for the culverts. ▪ The request is to construct culverts for each and every flooding areas. But in this project cannot construct all the culverts according to allocated funds. ▪ The three requests are for provide new culverts. But when after considered the locations, technically decided to construct another relevant places. ▪ The request is to construct a new culvert, but technically decided to construct it another suitable place. ▪ The request to construct the road surface more rough by using asphalt or concrete on the sharp bend areas. But there is no such mechanism available in technically justifiable for making the road surface rough.

Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
					<p>Alternatively it has decided to increase the road width at each and every sharp bends which are risky.</p> <ul style="list-style-type: none"> ▪ The request is to construct new culverts, but technically recommended to construct new culverts according to suitable locations. ▪ The request is to construct new culverts, but technically recommended to construct new culverts according to suitable locations.
PU 02	235	212	07	16	<ul style="list-style-type: none"> ▪ The request is to increase the number of culverts due to flood inundation area. But this is not convenient according to the technical aspects as well as the due to limited funds, it cannot be accepted. ▪ The request is to widen the culvert. But due to lack of provisions it cannot be accepted. ▪ The request is to Asphalt another 25m at the Junction which is connectivity roads. But due to lack of BOQ quantities it cannot proceed. The request/suggestion is to reconstruct the anicut due to insufficient width. But it is responsible of Department of Agriculture. ▪ The request is to provide a hump near the Mohottimulla School. ▪ The request is to increase the road width. But there is no road widening in this project. ▪ The request is to provide vehicle weight confining system. But it cannot provide due to lack of funds.
PU 03	155	124	00	31	<ul style="list-style-type: none"> ▪ The request is to reconstruct the anicut due to insufficient width. But it is responsible of Department of Agriculture. ▪ The request is to provide a hump near the Mohottimulla School. ▪ The request is to increase the road width. But there is no road widening in this project.


Package	No. of Complaints/ Requests/ Suggestions	No. of action taken	No. of rejected due to reasonable matter	No. of cases Solution In-progress	Remarks
					<ul style="list-style-type: none"> ▪ The request is to provide vehicle weight confining system. But it cannot provide due to lack of funds.

Annex: 11.3






Construction Developments-Kurunegala District







Photographs before and after Roads/Culverts/Causeways/Bridges/side drains/house access etc
(taken in this year)

Package	RD ID	Description	Photographs before	Photographs after
KU 01	34	Safety at 0+500		
KU 01	36	Culvert at 6+200		
KU 01	76	Culvert at 2+900		
KU 01	81	Safety at 3+610		

KU 01	86	Culvert at 3+320		
KU 01	98	Safety at 1+880		
KU 01	50	Culvert at 5+030		
KU 01	76	Culvert at 2+900		
KU 02	RD ID 41	Request-construct a protection wall around the Bo Tree located 0+000 Solution-A Kerb wall was provided		

KU 02	RD ID 41	reconstruct culvert for the by road access- 0+820 LHS Solution- by road access was reconstructed.		
KU 02	RD ID 41	Request- asphalt or concrete the curve 1+430 RHS Solution- curve widening done.		
KU 02	RD ID 41	Request- provide protection wall 1+516-1+540 LHS Solution - U-Drain provided		
KU 02	RD ID 41	Request- provide retaining wall at Ch. 3+795 RHS- Solution- kerb wall was raised.		
KU 02	RD ID 41	2+735-2+765 LHS Kerb drain provided		

KU 0 2	RD ID 41	Request a protection wall at 1+750 RHS Solution-A toe wall was provided		
KU 03	37	Kerb wall at 1+260		
KU 04	92	Culvert at 0+450		
KU 04	92	Hard shoulders 4+650		
KU 04	92	Safety at 9+300		

KU 04	07	Culvert at 0+700		
KU 04	07	Culvert at 1+100		
KU 04	07	Culvert at 1+710		
KU 04	92	6+600		

Annex: 11.4

Case study

Package : KU 02 Division: Alawwa

Road name : Boyawalana, Nawathalwatta Wewala school road across Habaragasinna to Madurapitiya Road- (RD ID – 01)

Location : 3+200 RHS

Requested by: Mr. George Fernando

Issue : Demolition of his old building and degradation of his land due to flooding

Introduction : Mr. George Fernando is a villager at Boyawalana, who is the owner of a Mill (Old building) located at Ch.3+200 RHS adjacent to the Rd ID-01. His mill is consisted with a Grain grinding machine and Chilly grinding machine which are powered by three phase electricity. Also a well located behind to this building which they use for drinking water purpose .In other way the Land owner's house has located behind this old building and also the access road of this house is located adjacent to this old building. Another important fact is, a foot path is connected to that house access which is using by villagers to enter the iRoad for fulfill their day today accessibility.



Owner's mill(Old building)



Foot path use by villagers to enter i road

Problem identification: Together with construction works of the road Id.01, by iRoad programme, the problem was become significant, because of increased height of the road, a huge amount of runoff water coming along the road No 01 has been flown along the lead away which is crossed George's land, directly without flowing along the side drains of the mentioned road.

Then he has informed his grievance in writing to the project manager of KDAW on 30/05/2018 for a suitable solution to prevent damages of his land (Ch.3+200 RHS) where the leader way already existed. Furthermore, he afraid that the old building which was located very near to the water path would be affected due to the heavy runoff water.

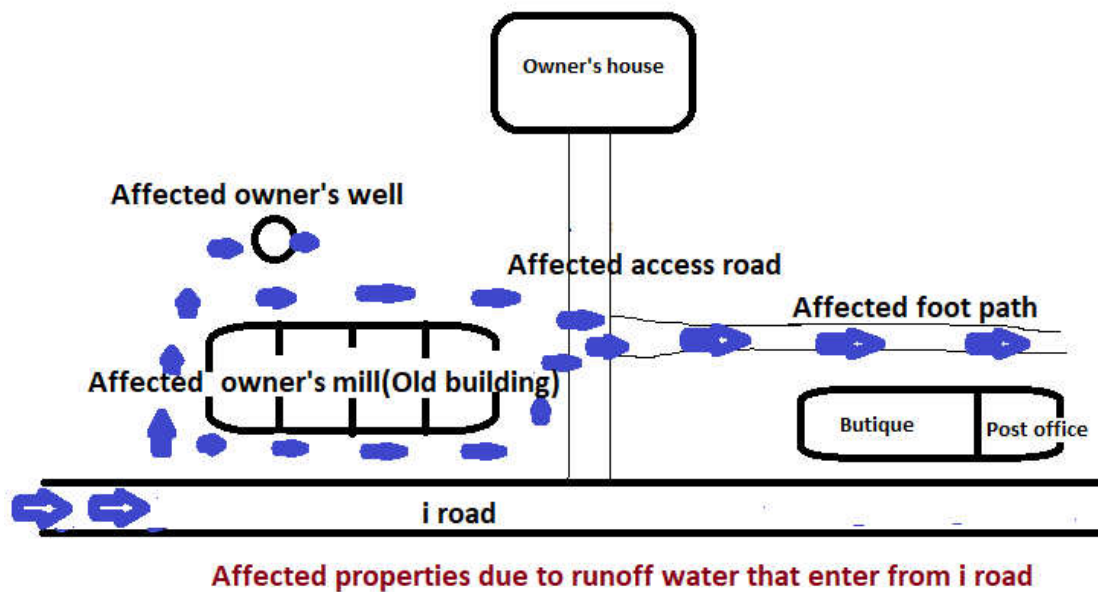
Observation:

According to the Project manager's letter (EMAP/271) on 02nd July 2018, Employer, Engineer, Contractor and Mr.George jointly observed the above location to take a suitable solution immediately.



Team has observed so many critical environmental and social issues as listed below,

1. One side of the old building has been washed out and the stability of the building is in a risk level as this building was already in dilapidated condition.
2. Well water was mixed with mud and cannot be used for drinking.
3. Land owner's whole land was affected due to this huge amount of runoff water.
4. Access road of the owner's house and foot path adjacent to the old building has been inundated and cannot be used during rain. Under this situation, few families of this area have faced for many constrains, because they couldn't use usual access to post office and market. In other words their day-to-day works affects during heavy rains due to new condition of the road.

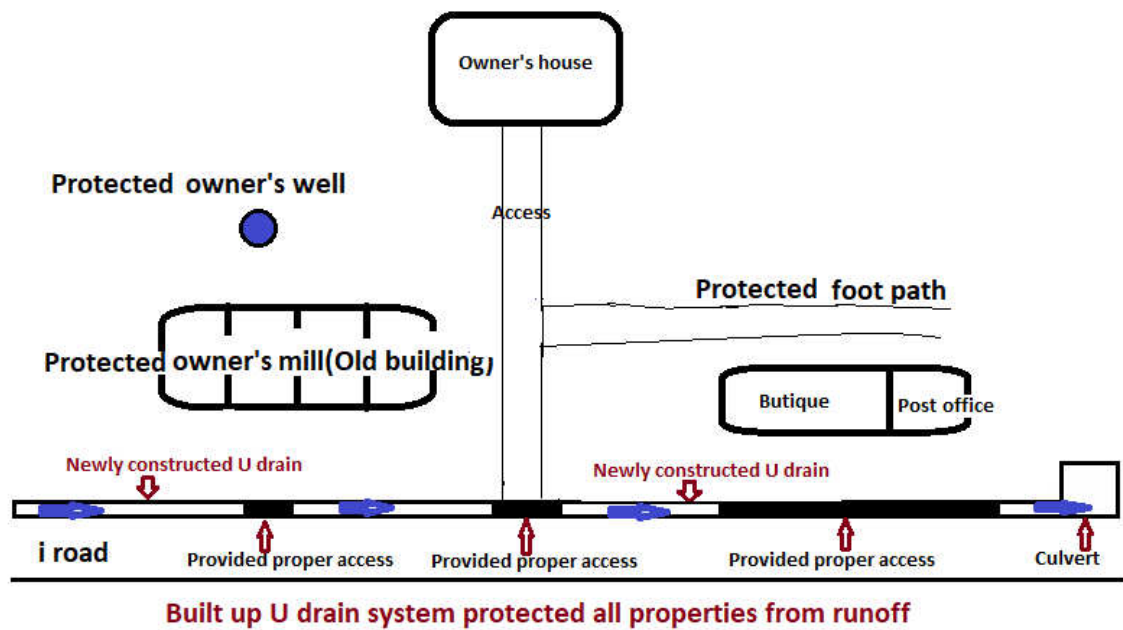


Problem analyzing and solution:

However, before starting construction works in this iRoad, runoff water has been flown at Ch.3+200 in two ways,

1. Large amount of water comes from upstream has been flown along the road and it has been crossed the road at Ch.3+200 and that water was directed to the culvert which is located left side of the road.
2. Another small portion of runoff water was entering to his land (Right side in Ch.3+200) and flowing behind his old building slowly.

Therefore, Engineer decided to construct a concrete U-drain to direct runoff water to the adjacent culvert with the consent of above person and adjacent villagers on 05/09/2018.



Outcome: Finally a regular U-drain system was built up with suitable accesses for the Old building, House access road, Boutique and Post office. Therefore, the above problem was resolved with the satisfaction of all the parties affected and then, their life pattern turned in to the normal.



Constructed u drain with relevant accesses



Before state



After state

Prepared by: Mrs. Kalhari Harsha (DSESA-Kurunegala)

Annex:11.5




Specimen of Taking over Certificate (TOC)

102/01/43/18

Pyunghwa Engineering Consultants Ltd
In association with
Resource Development Consultant (Pvt.) Ltd



ADB Funded – SRI – Integrated Road Investment Program
Consulting Services for Project Implementation including Construction Supervision, Contract Management and other activities in North Western and North Central Provinces.

TAKING OVER CERTIFICATE ROAD DEVELOPMENT AUTHORITY INTEGRATED ROAD INVESTMENT PROGRAM NORTH WESTERN PROVINCE		
CONTRACT NO: RDA/ADB/i Road (NW)NCB/CP-RR (KU2)		Original-2
Employer: Road Development Authority "Maganeguma Mahamedura", 216, Denzil Kobbekaduwa Mawatha, Koswatta, Battaramulla, Sri Lanka.	Engineer: Pyunghwa Engineering Consultants Ltd in association with Resource Development Consultants Ltd, No. 180/6, Pubudu Mawatha, Colombo Road, Kurunegala.	Contractor: KDAW-BMG Joint Venture, No.8/16, Thalapatipitiya Road, Nugegoda.
Issued Under: General Conditions of Contract- Sub Clause 10.2 Twelve (12) month Defects Notification Period (S.C.I.1.1.3.7 of G.C.C) and the three (3) year Performance Based Maintenance Period (S.C.I.11.9 of P.C.C Part B)		
Date of Commencement: 01 Aug. 2016		
Date of Taking Over: 15 Mar. 2019	Defects Notification Period: From 16 Mar. 2019 to 15 Mar. 2020	Performance Based Maintenance Period: From 16 Mar. 2019 to 15 Mar. 2022
Part of the Permanent Works Taken Over: Mahakeliya Randeniya Junction to Randeniya, Wellagala Road (Road ID 66) – 3.660km (PS)		
Attachments	Date to be completed	Remarks
1. Outstanding Works	25 March 2019	Annexure-A
2. Outstanding As Built Drawings	30 March 2019	Annexure-B
3. Outstanding Environmental Issues	25 March 2019	Annexure-C
Contractor's Representative:  Name: P.M.S.K.P. Pathiraja Designation: Contractor's Representative Date: 15-03-19	Engineer's Representative:  Name: Inderjeet Singh Gill Designation: Team Leader Date: 15/3/19	Employer:  Name: R.M.P. Rathnayaka Designation: Project Director Date: 15-03-2019

CC. Original 1 – Contractor
Original 2 – Employer
Original 3 – Engineer

No. 180/6, Pubudu Mawatha,
Colombo Road,
Kurunegala

Telephone/Fax: 037 2225484
E-mail: ipic02road@gmail.com

Contract Package : HMA/ACB/Road(NW)/NCR/CP-RR (KU2)
 Road ID & Name : Mahakeliya Bandeniya Junction to Bandeniya, Wellagala Road
 (Road ID 66)
 Actual Length of the Road : 3.660 km
 Date of Inspection : 15-Mar-2019
 I-ROADS / NORTH WESTERN PROVINCE

1. OUTSTANDING WORKS

Annexure-A

No	Chainage	Outstanding Work	Remarks
Drainage Works			
1	0+297L, 3+548L, 2+820L, 2+860L, 2+870L, 3+310 L	Balance Accesses to be done	
2	0+300 LHS	By road Access to be reconstructed	Additional work
3	2+205 RHS	Access level to be adjusted with Culvert Invert level	Additional work
4	3+465-3-480 RHS	U Drain to be provided	Additional work
Structural Work			
1	3+065	Bridge to be reconstructed	Additional work
Pavement Work			
1	1+820 LHS, 3+120 RHS	Concrete hard shoulder	Additional works
Incidental Works			
1	0+000-3+660 B/S	Balance sign boards to be fixed	
2	0+000-3+660 B/S	Culvert numbering to be done	

2. OUTSTANDING ASBUILT DRAWINGS




Annexure-B

No	Chainage		Drawing No	Remarks
	From	To		
1	0+000	3+660	-	Plan & profile drawing, Cross sectional drawing, Structural drawing, Road marking & signage drawing.

3. OUTSTANDING ENVIRONMENTAL ISSUES

Annexure-C

No	Chainage	Outstanding Issues	Remarks
1	0+220 LHS	Stock yard to be rehabilitated and Certified satisfaction letter of land owner to be obtained.	
2	1+950 LHS	Stock yard to be rehabilitated and Certified satisfaction letter of land owner to be obtained.	
3	3+300 LHS	Stock yard to be rehabilitated and Certified satisfaction letter of land owner to be obtained.	
4	3+520 RHS	House crack issue to be closed with Mrs. Thamara Kumari in writing.	

	Contractor's representative	Engineer's representative	Project Engineer (RDA)
Name	P.M.S.K.P. Pathiraja	M.V.T.Udoranga	W.L.D. Botheju
Designation	Contractor's representative	Construction Engineer	Project Engineer
Signature			
Date	15.03.2019	15.03.2019	15.03.2019

Annex:11.6

Land Donation/Consent Letters (NCP Registry only)-As at end of June 2019

District	Contract Package	Road ID	Name of Road	S. No	Chainage		Side of road	Type of owners hip (Govern ment/ private)	Reason for additional land requirement	Name and address of present occupant
Project 4 – North Central Province										
Anuradh apura	AP1	41	Sucharithgama-Yahalegama Rd	LD/AP1/41/1	3+910		RHS	P	Lead away	
		41	Sucharithgama-Yahalegama Rd	LD/AP1/41/2	3+977		RHS	P	Lead away	
		39	4 Elajunction-Prasangawewa hospital rd	LD/AP1/39/01	2+820		LHS	P	Lead away	W.Suaris,Hospital Rd, Parasangawewa
		63	9 Ela junction janahitha junction 7 ela thuruwila	LD/AP1/63/01	0+697		LHS	P	Lead away	W.M.Gunathilaka Banda Athuruwela, Nachchaduwa.
		65	Nelubewa Bidhunkada Samanala road	LD/AP1/65/01	1+458		LHS	P	Lead away	S.Niroshani, 51,Kudawewa,Hidhogama
		65	Nelubewa Bidhunkada Samanala road	LD/AP1/65/02	3+840		LHS	P	Lead away	K.T.Indhika dharshani, 45,Kudaweaw , Hidhogama
		65	Nelubewa Bidhunkada Samanala road	LD/AP1/65/03	2+888		RHS	P	Lead away	L.R.Somawathi, 19,Bidhunkada Hidhogama
		65	Nelubewa Bidhunkada Samanala road	LD/AP1/65/04	0+698		LHS	P	Lead away	H.A.Indra Rohini , 20,Samupakara pathumaga
		10	Mahwelithenna Ralapanawa janapadhaya road	LD/AP1/10/01	3+533		LHS	P	Lead away	B.R.JAYATHILAKA 12,Ralapanawa janapadaya Nochchiyagama

	AP1	10	Mahwelithenna Ralapanawa janapadhaya road	LD/AP1/10/02	2+993		LHS	P	Lead away	I.G.Dharmadasa 313,Ralapanawa janapadaya Nochchiyagama
		36	Yaya 04 junction Bogas junction Kiralapetiyawa Halambe road	LD/AP1/36/01	5+855		RHS	P	Lead away	W.Sriyalatha 76A,Kiralapetiyawa, Mahawilachchiya
		36	Yaya 04 junction Bogas junction Kiralapetiyawa Halambe road	LD/AP1/36/02	5+855		RHS	P	Lead away	G.L.Predheepa chandhani Yaya5, Right Bank,Paymaduwa
	AP2	27	Kudawatagala-Dambuluhalmillew a Rd	LD/AP2/27/1	2+300		LHS	P	Lead away	B.G.Somapala, 78/8 Narangaswewa Dewahoowa
		28	Ulpothagama Junction Moroththegama Rd	LD/AP/2/28/1	3+815		RHS	P	Lead away	G.D.Nimal Kulathunga Narangaswewa,Devahuwa
		27	Kudawatagala Dambuluhalmillew a Rd	LD/AP/2/27/2	9+225		LHS	P	Road Improvement	S.M.Wimalawathi Ranawa, Galkiriyagama
		60	Thammannawa junction Kubukkuwewa	LD/AP2/66/01	6+550		RHS	P	Leadaway	D.M.S.Muthubanda, 338.Hunupalagama,Siyabelawa negampaha
		48	Aurwedic Hospital to Nawagattegama	LD/AP2/48/01	2+860	2+910	RHS	P	Road Improvment	R.M.Nuwan Kumara, Nawagattegama,Thambuttegama
		48	Aurwedic Hospital to Nawagattegama	LD/AP2/48/02	2+750	2+860	RHS	P	road improvement	R.Nandhawathi, Nawagattegama,Thambuttegama
		48	Aurwedic Hospital to Nawagattegama	LD/AP2/48/03	2+750	2+860	LHS	P	road improvement	R.M.H.Jayathissa, Nawagattegama,Thambuttegama
		67	Kalawewa aukana road	LD/AP2/67/01	4+383	4+400	RHS	P	road improvement	W.K.K.N.H.M.D.Bandara Temple Rd,Aukana
		67	Kalawewa aukana road	LD/AP2/67/02	4+383	4+400	RHS	P	road improvement	R.M.Sarath Bandaranayaka Temple Rd, Aukana
	AP3	5	Matale Junction-Samagipura Rd	LD/AP3/5/1	1+398-	1+415	RHS	P	Road Improvement	L.P.S.Rujith Kumara, 511-B, Yasapala Herath Mawatha, Thennayankulama,Mihintale.
		5	Matale Junction-Samagipura Rd	LD/AP3/5/2	1+420-	1+444	RHS	P	Road Improvement	S.S Shantha, 510, Henawatha Rd, Thannayankulama, Mihintale.
		5	Matale Junction-Samagipura Rd	LD/AP3/5/3	1+506-	1+535	RHS	P	Road improvement	B.Charlot,508, Yasapala Herath Mawatha, Thennayankulama,Mihintale

AP3	5	Matale Junction-Samagipura Rd	LD/AP3/5/4	1+535-	1+575	RHS	P	Road improvement	P.G.Cyril Wimaladasa, Yasapala Herath Mawatha, Thennayankulama,Mihintale
	5	Matale Junction-Samagipura Rd	LD/AP3/5/5	1+582-	1+618	RHS	P	Road improvement	D.K.S.Karunaratna,503, Yasapala Herath Mawatha, Thennayankulama,Mihintale
	5	Matale Junction-Samagipura Rd	LD/AP3/5/6	1+501-	1+565	LHS	P	Road improvement	E.A.R.Lanka Amarasinghe, 435/A, Yasapala Herath Mawatha, Thennayankulama,Mihintale
	5	Matale Junction-Samagipura Rd	LD/AP3/5/7	1+619-	1+641	RHS	P	Road improvement	S.A. Rasika Hemantha, 13/457,Nidahas mawatha, Pulleys junction,
	5	Matale Junction-Samagipura Rd	LD/AP3/5/8	1+641-	1+660	RHS	P	Road improvement	D.I.Dharmasiri, 502, Yasapala Herath Mawatha, Thennayankulama,Mihintale
	9	Velankulama Junction-Alappankulama Rd	LD/AP#/9/1	1+714		LHS	P	Lead away	K.B.Nandane Ramyalatha, Mahahakirindagama, Mihintale.
	6	Kurundamkulama School via Kalattewa Matale Junction rd	LD/AP3/6/1	2+047		LHS	P	Lead away	W.M.T.Menike, 29, Track.6,Kurundamkulama, Anuradapura.
	6	Kurundamkulama School via Kalattewa Matale Junction rd	LD/AP3/6/2	2+190		LHS	P	Lead away	Wimala Gunasekara, 27, Track.6,Kurundamkulama, Anuradapura
	6	Kurundamkulama School via Kalattewa Matale Junction rd	LD/AP3/6/3	2+716-	2+774	RHS	P		All land owners not agreed to donet the required land portion
	6	Kurundamkulama School via Kalattewa Matale Junction rd	LD/AP3/6/4	2+705-	2+750	LHS	P		
	6	Kurundamkulama School via Kalattewa Matale Junction rd	LD/AP3/6/5	2+750-	2+820	LHS	P		
	6	Kurundamkulama School via Kalattewa Matale Junction rd	LD/AP3/6/6	2+746-	2+770	RHS	P		
	70	Katukeliyawa-Ihalahalmillewa-Sivalakulama Rd	LD/AP3/70/1	1+693	1+714	RHS	P	Lead-away	K.Sewwa Pahalalahallmillawa,Siwalakulama

AP3	60	A09 Road Labunoruwa Muriyakadawala Road	LD/AP3/60/01	8+549		RHS	P	Lead-away	K.Leelarathna Kattamurichchana, Maradankadawala
	60	A09 Road Labunoruwa Muriyakadawala Road	LD/AP3/60/02	11+590		RHS	P	Lead-away	K.Jayawardhana Sandhanankulama,Labunoruwa
	71	Palugaswewa Udakadawala Road	LD/AP3/71/01	1+421	1+481	LHS	P	Land donation	P.B.Thilakarathna Udakadawala , Palugaswewa
	71	Palugaswewa Udakadawala Road	LD/AP3/71/02	1+481	1+520	LHS	P	Land donation	G.Dharmarathna Udakadawala , palugawewa
	71	Palugaswewa Udakadawala Road	LD/AP3/71/03	0+835			P	Lead-away	Reservation land
	71	Palugaswewa Udakadawala Road	LD/AP3/71/04	1+154			P	Lead-away	Reservation land
	60	A09 Road Labunoruwa Muriyakadawala Road	LD/AP-03/60/2					Leadaway	Thilani Kumarathunga
	60	A09 Road Labunoruwa Muriyakadawala Road	LD/AP-03/60/3					Lead-away	S.Gunarathna
	60	A09 Road Labunoruwa Muriyakadawala Road	LD/AP-03/60/3					Lead-away	K.jayawardhana
	60	A09 Road Labunoruwa Muriyakadawala Road	LD/AP-03/60/4					Lead-away	D.G.S.A.Gunathilaka
	60	A09 Road Labunoruwa Muriyakadawala Road	LD/AP3/60/05	4+200		RHS	P	Lead - away	K.Chandrasuriya
	6	Kurundankaulama School Via Kalaththewa Matale Junction Road	LD/AP3/06/07	0+000	0+015	RHS	P	Land donation	W.M.Premathilaka, Unit07 , kurundhankulama,Anuradhapura
	6	Kurundankaulama School Via Kalaththewa	LD/AP3/06/08	0+025	0+028	LHS	P	Land donation	H.M.Sriyani Manike, unit 10 kurundhankulama Anuradhapura

AP3		Matale Junction Road							
	6	Kurundankaulama School Via Kalaththewa Matale Junction Road	LD/AP3/06/09	0+038	0+050	LHS	P	Land donation	W.P.Shalika Madhuwanthi, Near the water tank kurundhankulama
	6	Kurundankaulama School Via Kalaththewa Matale Junction Road	LD/AP3/06/10	2+820	2+880	LHS	P	Land donation	Y.G.N.Jayasingha, 30/6 Kurundhankulama
	29	Yakalla Palugollagama Upuldeniya Road	LD/AP3/29/01	5+568		LHS	P	Land donation	R.Gamini rupasingha , Wenbuwewa siwalakulama
	25	Laksirigama puwakpitiya kuda rabewa road	LD/AP3/25/01	0+634.6		LHS	P	Lead - away	W.R.K.Chamara wickramanayaka, Benlankadawala,palugaswewa
	29	Yakalla palugollagama Upuldeniya	LD/AP3/29/02	5+568		RHS	P	Lead - away	R.Gamini rupasingha Wenbuwewa siwalakulama
	5	Matale Junction Samagipura Road	LD/AP3/05/09	2+297	2+320	RHS	P	Lead - away	U.Somawathi No,390 Henawaththa,Pulleyar junction
	5	Matale Junction Samagipura Road	LD/AP3/05/10	0+060	0+092	RHS	P	Road Improvement	L.G.Sunil shantha No.01 Samagimawatha ,Mathale junction
	5	Matale Junction Samagipura Road	LD/AP3/05/11	1+084	1+145	RHS	P	Road Improvement	W.H.Premarathna No,33 samagipura,matale junction
	5	Matale Junction Samagipura Road	LD/AP3/05/12	1+223	1+233	RHS	P	Road Improvement	S.M.S.Bandhulasena No34/B Samagipura
	5	Matale Junction Samagipura Road	LD/AP3/05/13	0+000	0+060	RHS	P	Road Improvement	R.R.Hettihewage Mathale road,kurundhankulama, Anuradhapura
	6	Kurundankaulama School Via Kalaththewa Matale Junction Road	LD/AP3/06/07	1+974	1+047	RHS	P	Road Improvement	M.M.Mallika Siriwardhana No.39 Kurundhankulama,Anuradhapura

	AP3	60	A-9 Labunoruwa Muriyakadawala Rd	LD/AP-03/60/06						
		60	A-9 Labunoruwa Muriyakadawala Rd	LD/AP-03/60/11						
		06	Kurundankulama School via Matale Rd	LD/AP-03/06/11						
	AP4	16	A9 KarapikkadaKuruk kandegama Kebithigollewa	LD/AP4/16/1	1+820-1+900		RHS	P	Lead away	M.A.somadasa Akkarakoduwa Kumbidampirijawa Medawachchiya
		32	Ratmalgahaweawa Rd- Kurukkuragama Junc.	LD/AP4/32/1	3+965		RHS	P	Lead away	A.J.M.Sudubanda Akkarakoduwa Kumbidampirijawa Medawachchiya
		31	Kahatagasdigiliya-Kainattama Rd	LD/AP4/31/01	1+110		RHS	P	Lead away	K.Dingiribanda Akkarakoduwa Kumbidampirijawa Medawachchiya
		31	Kahatagasdigiliya-Kainattama Rd	LD/AP4/31/02	1+951		LHS	P	Lead away	K.Karunaratna Akkarakoduwa Kumbidampirijawa Medawachchiya
		32	Rathmalgahawewa Road Kurukkuragama Junction to Hettikattiya Road	LD/AP4/32/01	3+965		LHS	P	Lead-away	
		31	Kahatagasdhigiliya Kainattama Road	LD/AP4/31/01	1+110			P	Lead-away	
		31	Kahatagasdhigiliya Kainattama Road	LD/AP4/31/01	1+951			P	Lead-away	
		14	Ihalagama junction Muslim diulwewa Road	LD/AP4/14/01	1+790		RHS	P	Lead-away	U.T.Wijerathna, Galkadawala kedewa
		14	Ihalagama junction Muslim diulwewa Road	LD/AP4/14/02	3+005			P	Lead-away	
	AP4	15	Pihibiyagollewa 9th post Ambagahawewa Road	LD/AP4/15/01	1+250			P	Lead-away	

		17	Puhudiula Junction to Galkadawala Road	LD/AP4/17/01	1+790		RHS	P	Lead-away	U.T.Wijerathna, Galkadawala kedewa
		50	40 Kolaniaya Bus Halt Nawa gammanaya road	LD/AP4/50/01	0+496.5		RHS	P	Lead-away	J.K.A.Piyathissa, 120/A 40 kolaniya padaviya
		50	40 Kolaniaya Bus Halt Nawa gammanaya road	LD/AP4/50/02	1+180.7		RHS	P	Lead-away	G.M.Wijayawansa, 131/A 7Kolaniya Padavi parakkramapura
		52	Mailagas junction Dhaluggala Jayanthi Road	LD/AP4/50/03	2+980		RHS	P	Lead-away	S.D.Sumanawathi, 1276,Dhaluggala Mahasenpura
		52	Mailagas junction Dhaluggala Jayanthi Road	LD/AP4/52/01	2+010		RHS	P	Lead-away	R.M.Dhammika Nishantha, 1205 Dhaluggala Mahasenpura
		50	40 Kolaniaya Bus Halt Nawa gammanaya road	LD/AP4/50/04	1+180.7		RHS	P	Lead-away	M.A.Peiris, 24 7 kolaniya
		16	A9 Karapikkada Kurukkandegama Kebithigollewa	LD/AP4/16/02	4+319		LHS	P	land donation	K.Pangnasara thero, Kurukkandhegama temple,madawachchiya
Polonna ruwa	PO1	10	Track 32 Temple-33 Bisokotuwa Junk Road	LD/PO1/10/1	1+140-1+304		RHS	P	Road improvement	R.M.Bisomenike, Track.32,No46,Konduruwawa,Attan akadawala,
		10	Track 32 Temple-33 Bisokotuwa Junk Road	LD/PO1/10/2	0+350-0+404		RHS	P	Road improvement	K.G.Alahakoon Track.32,No46,Konduruwawa,Attan akadawala
		10	Track 32 Temple-33 Bisokotuwa Junk Road	LD/PO1/10/3	0+404-0+422		RHS	P	Road improvement	K.G.Wijeratna, Track.32,No46,Konduruwawa,Attan akadawala
		10	Track 32 Temple-33 Bisokotuwa Junk Road	LD/PO1/10/4	1+389+1+569		RHS	P	Road improvement	W.M.Upali Wijesinghe, Track.32,No50,Konduruwawa,Attan akadawala
		10	Track 32 Temple-33 Bisokotuwa Junk Road	LD/PO1/12/5	1+681-1+855		LHS	P	Road improvement	T.A.G.Piyasena, Kaduruwewa,Mahayaya,Attanakada wala

PO1	10	Track 32 Temple-33 Bisokotuwa Junk Road	LD/PO1/10/6	1+865-1+940		LHS	P	Road improvement	A.M.Tikiri Banda 62/1, Kaduruwewa, Mahayaya, Attanakadawala
	3	Attanakadawara-Pokunugala Rd	LD/PO1/3/1	1+865-1+940		LHS	P	Road improvement	D.S.MRanasinghe Banda Track.29, No.49, Attanakadawala, Polonnaruwa
	3	Attanakadawara-Pokunugala Rd	LD/PO1/3/2	2+742		LHS	P	Lead away	D.S.M.Rtnapala Track 26, No.14, Attanakadawara, Polonnaruwa
	3	Attanakadawara-Pokunugala Rd	LD/PO1/3/3	0+535-0+560		LHS	P	Road improvement	G.K.Ratnayake Track.26, No10, Attanakadawala
	3	Attanakadawara-Pokunugala Rd	LD/PO1/3/4	0+635-0+679		RHS	P	Road improvement	J.A.Pathmasiri Jayasinghe Track.26, No12-R, Attanakadawala
	3	Attanakadawara-Pokunugala Rd	LD/PO1/3/5	0+679-0+695		RHS	P	Road improvement	M.A.Leelawathi Track.26, No19, Attanakadawala
	3	Attanakadawara-Pokunugala Rd	LD/PO1/3/6	0+610-0+710		LHS	P	Road improvement	R.P.Kamalawathi Track.26, No11, Attanakadawala
	3	Attanakadawara-Pokunugala Rd	LD/PO1/3/7	1+030-1+065		LHS	P	Road improvement	S.S.Ariyadasa, Track.26, No26, Attanakadawala
	3	Attanakadawara-Pokunugala Rd	LD/PO1/3/8	2+965-3+085		LHS	P	Road improvement	A.M.Lokubanda Track.26, No41, Attanakadawala
	3	Attanakadawara-Pokunugala Rd	LD/PO1/3/9	3+085-3+190		LHS	P	Road improvement	A.M.T.Abeyratna, Track.26, No41, Attanakadawala
	8	Orubendisiyambalawa Village Rd	LD/PO1/8/1	1+220-1+295		LHS	P	Road improvement	B.G.Somawathi, 2/44, Orubendisiyambalawa
	8	Orubendisiyambalawa Village Rd	LD/PO1/8/2	1+500-1+526		RHS	P	Road improvement	W.M.R.M.Ranjith Wijebandara 2/49, Orubendisiyambalawa
	8	Orubendisiyambalawa Village Rd	LD/PO1/8/3	1+530-1+540		LHS	P	Road improvement	A.G.Nimal Wijebandara 2/59, Orubendisiyambalawa
	8	Orubendisiyambalawa Village Rd	LD/PO1/8/4	1+585-1+605		LHS	P	Road improvement	M.D.Jayasinghe, 2/60. Orubendisiyambalawa
	8	Orubendisiyambalawa Village Rd	LD/PO1/8/5	1+600-1+630		RHS	P	Road improvement	N.A.Premaratana 2/52 Orubendisiyabalawa
	8	Orubendisiyambalawa Village Rd	LD/PO1/8/6	1+600-1+653		RHS	P	Road improvement	R.M.Wijeratna 2/53 Orubendisiyabalawa

PO1	8	Orubendisiyambalawa Village Rd	LD/PO1/8/7	1+660-1+680		LHS	P	Road improvement	Pradeep Priyantha 2/52 Orubendisiyabalawa
	8	Orubendisiyambalawa Village Rd	LD/PO1/8/8	1+694-1+706		LHS	P	Road improvement	N.A.G.Somapala 2/52 Orubendisiyabalawa
	8	Orubendisiyambalawa Village Rd	LD/PO1/8/9	1+706-1+746		LHS	P	Road improvement	Orubendisiyabalawa
	8	Orubendisiyambalawa Village Rd	LD/PO1/8/10	1+746-1+776		LHS	P	Road improvement	W.M.Nandawathi 57 Orubendisiyabalawa
	14	Kadawalawewa Village Rd.	LD/PO1/14/1	0+815-0+860		LHS		Road improvement	PO/Kadawalawewa school, Jayanthipura,polonnaruwa
	14	Kadawalawewa Village Rd.	LD/PO1/14/2	0+985-1+060		RHS	P	Road improvement	P.L.J.Perera 527/A 23 Mile post, Gaminee school Rd, Jayanthipura
	14	Kadawalawewa Village Rd.	LD/PO1/14/3	0+740 0+820		RHS	P	Road improvement	K.R.Gaminee Chandrasekara,4,Perakum Uyana Polonnaruwa
	21	Parakrama Samudraya-Ambanganga Village Rd	LD/PO1/21/1	0+740 0+820		RHS	P	Road improvement	Gamini Chandrasekara 4, Perakum Uyana Polonnaruwa
	21	Parakrama Samudraya-Ambanganga Village Rd	LD/PO1/21/2	0+820 0+835		RHS	P	Road improvement	Kamal Anuranga 22/1 Prakkrama samudraya,polonnaruwa
	21	Parakrama Samudraya-Ambanganga Village Rd	LD/PO1/21/3	00+835 0+857		RHS	P	Road improvement	G.R.A.N.Galgamuwa
	21	Parakrama Samudraya-Ambanganga Village Rd	LD/PO1/21/4	0+857-0+898		RHS	P	Road improvement	G.A.R.N.Galgamuwa 22,Prakrama Samudraya Polonnaruwa
	21	Parakrama Samudraya-Ambanganga Village Rd	LD/PO1/21/5	0+898-0+960		RHS	P	Road improvement	K.D.Udaya kumara
	21	Parakrama Samudraya-Ambanganga Village Rd	LD/PO1/21/6	0+960-1+008		RHS	P	Road improvement	D.M.Dissanayaka

PO1	21	Parakrama Samudraya-Ambanganga Village Rd	LD/PO1/21/7	1+076-1+090		RHS	P	Road improvement	R.H.Wijesiri
	21	Parakrama Samudraya-Ambanganga Village Rd	LD/PO1/21/8	1+270-1+340		RHS	P	Road improvement	Tissa weerakkodi Prakrama Samudraya Polonnaruwa -
	21	Parakrama Samudraya-Ambanganga Village Rd	LD/PO1/21/9	1+360-1+640		RHS	P	Road improvement	Lal Ranjith Kumara Pabnnala 31,Parakrama Samudraya Polonnaruwa
	21	Parakrama Samudraya-Ambanganga Village Rd	LD/PO1/21/10	1+640-2+060		RHS	P	Road improvement	ANULA R.M.Rathnayaka, Jagath Aruna keerthi
	20	Onagama main road - galkoriya cemetry	LD/PO1/20/01	1+150 1+230		LHS	P	ROAD Improvement	A.M.Wickramasinghe 71,Monarathenn Palugasdamana
	20	Onagama main road - galkoriya cemetry	LD/PO1/20/02	1+150 1+230		LHS	P	Road Improvement	A.M.Wickrasingha
	20	Onagama main road - galkoriya cemetry	LD/PO1/20/03	1+330-1+500		LHS	P	Road Improvement	A.Karunawathi
	20	Onagama main road - galkoriya cemetry	LD/PO1/20/04	1+330 1+500		LHS	P	Road Improvement	D.M.Senarathna
	20	Onagama main road - galkoriya cemetry	LD/PO1/20/05	1+660 1+720		LHS	P	Road Improvement	W.Srinika wijesingha
	3	Aththanakadawala pokunugala road	LD/PO1/03/11	2+800 2+900		LHS	P	Road Improvement	R.K.Weerasinghe 19,Track 11,Monarathenna D.J.P.Roshana
	10	Track32 temple - 33 bisokotuwa junction road	LD/PO1/10/07	0+270 0+410		LHS	P	Road Improvement	G.K.jayathilake Track 26, No.15 Attanakadawala
	3	Attanakadawala-Pokunugala Rd	LD/PO1/3/11	0+420 0+470		LHS	P	Road Improvement	S.P.Silva Tack 20,No 9, Attanakadawala
	10	Trak 32,templw 33Bisokoruwa Junc Rd	LD/PO1/10/7	0+535 0+560		LHS	P	Road Improvment	A.M.Abeysinghe 32/42,Kaduruwela Attanakadawala

PO-02	28	BOP 316,Karuwalagas Junction- Chandanapokuna	LD/PO2/28/1	0+000- 0+093		LHS	P	Road improvement	U.G.Tikiribanda
	28	BOP 316,Karuwalagas Junction- Chandanapokuna	LD/PO2/28/2	0+093- 0+145		LHS	P	Road improvement	U.G.P.Kumara
	28	BOP 316,Karuwalagas Junction- Chandanapokuna	LD/PO2/28/3	0+145- 0+205		LHS	P	Road improvement	W.Rabulwalla
	28	BOP 316,Karuwalagas Junction- Chandanapokuna	LD/PO2/28/4	0+523- 0+540		LHS	P	Road improvement	-
	28	BOP 316,Karuwalagas Junction- Chandanapokuna	LD/PO2/28/5	0+678- 0+832		LHS	P	Road improvement	Kalubanda
	28	BOP 316,Karuwalagas Junction- Chandanapokuna	LD/PO2/28/6	0+832- 0+850		LHS	P	Road improvement	K.N.N.K.Jayasingha
	72	Dabarella- Sudupalama- Galamuna	LD/PO2/72/1	0+00 0+000		RHS	P	Road improvement	M.R.Wayalat
	72	Dabarella- Sudupalama- Galamuna	LD/PO2/72/2	0+000- 0+60		LHS	P	Road improvement	H.P.Wimalasena 117 BOP 317 Senanayakapura
	72	Dabarella- Sudupalama- Galamuna	LD/PO2/72/3	0+000- 0+100		RHS	P	Road improvement	M.R.Violet, Senanayakapura BOP 317
	72	Dabarella- Sudupalama- Galamuna	LD/PO2/72/4	0+060- 0+101		LHS	P	Road improvement	H.A.Wimalasena Senanayakapura BOP 317
	72	Dabarella- Sudupalama- Galamuna	LD/PO2/72/5	0+940 1+085		LHS	P	Road improvement	No need land donation Jayanthipura Primary School
	72	Dabarella- Sudupalama- Galamuna	LD/PO2/72/6	1+085 1+175		LHS	P	Road improvement	
	72	Dabarella- Sudupalama- Galamuna	LD/PO2/72/7	1+175 1+410		LHS	P	Road improvement	W.P.P.Bandara 119,Thalpotha,Debarella

PO2	72	Dabarella-Sudupalama-Galamuna	LD/PO2/72/8	1+410 1+480		LHS	P	Road improvement	G.H.M.Dingiribanda 118,Thalpotha,Debarella
	72	Dabarella-Sudupalama-Galamuna	LD/PO2/72/9	1+480 1+530 1+530 1+590		LHS	P	Road improvement	Samantha Sanjeewa 1117,Thalpotha,Debarella
	35	Kusum Pokuna-Pinpara Junction Rd	LD/PO2/35/1	0+210- 0+315		RHS	P	Road improvement	N.Gunawardhana
	35	Kusum Pokuna-Pinpara Junction Rd	LD/PO2/35/2	2+239 – 2+276		RHS	P	Road Improvement	G.M.Karunarathna
	41	Jayathugama Junction-Megollewa School Rd	LD/PO2/41/1	0+350- 0+500		RHS	P	Road improvement	K.G.L.Pemasiri Maha agahaswewa madirigiriya
	41	Jayathugama Junction-Megollewa School Rd	LD/PO2/41/2	0+800- 0+850		RHS	P	Road improvement	G.P.Dharmadasa Maha agahaswewa madirigiriya
	41	Jayathugama Junction-Megollewa School Rd	LD/PO2/41/3	1+00- 1+250		RHS	P	Road improvement	reser
	41	Jayathugama Junction-Megollewa School Rd	LD/PO2/41/4	1+300- 1+500		RHS	P	Road improvement	B.W.Thilakaratne Tack 9
	41	Jayathugama Junction-Megollewa School Rd	LD/PO2/41/5	1+800- 2+100		RHS	P	Road improvement	K.W.Ananada I.M.Bandara
	41	Jayathugama Junction-Megollewa School Rd	LD/PO2/41/6	3+050		RHS	P	Road improvement	H.G.Kiribanda, Mahaambagaswewa Medirigiriya
	73	Diulankadawala-aliwanguwa Junction road	LD/PO2/73/1	0+000- 0+250		LHS/ RHS	P	Road improvement	P.T.Letishiya brigat Fernando Ratmale Rd,Diulankadawala
	73	Diulankadawala-aliwanguwa Junction road	LD/PO2/73/2	0+400- 0+480		RHS	P	Road improvement	H.N.Sumanawathi Udaganawa Diulankadawala

PO2	73	Diulankadawala-aliwanguwa Junction road	LD/PO2/73/3	0+480-0+680		LHS/RHS	P	Road improvement	B.L.Rupasingha Ratmale Rd,Diulankadawala
	73	Diulankadawala-aliwanguwa Junction road	LD/PO2/73/4	1+200-1+350		RHS	P	Road improvement	H.W.A.Devit Ratmale Rd,Diulankadawala
	47	Dora deka junction-Paluwewa Rd	LD/PO2/47/1	1+220-1+270		LHS	P	Road improvement	S.G.Wasantha Herathkumara,Ulpothawewagama Hinguraggoda
	47	Dora deka junction-Paluwewa Rd	LD/PO2/47/1	1+270-1+340		RHS/LHS	P	Road improvement	W.T.Dissanayaka siriyawathi
	47	Dora deka junction-Paluwewa Rd	LD/PO2/47/1	1+420-1+470		RHS/LHS	P	Road improvement	A.B.N.S.Rathnayaka
	47	Dora deka junction-Paluwewa Rd	LD/PO2/47/1	1+470-1+650		LHS	P	Road improvement	G.sampath H.M.chandralatha
	45	Minneriya Central College Road	LD/PO2/45/1	1+010-1+030		LHS	P	Road improvement	Shantha Dhesappriya Akkara70,Hinguraggoda
	45	Minneriya Central College Road	LD/PO2/45/2	1+120-1+150		RHS	P	Road improvement	R.A.P.Ranjani Akkara70,Hinguraggoda
	45	Minneriya Central College Road	LD/PO2/45/3	2+950-2+966		LHS	P	Road improvement	H.A.S.C.K.Hettiarachchi Akkara70,Hinguraggoda
	45	Minneriya Central College Road	LD/PO2/45/3	2+950-3+000		LHS	P	Road improvement	Thamara kumari rathnayaka Akkara70,Hinguraggoda
	46	Hathamuna Bridge-Nugagahadamana Rd	LD/PO2/46/1	0+100-0+300		RHS/LHS	P	Road improvement	H.G.Wayalathnona Akkara 60 Colony,Giritale
	46	Hathamuna Bridge-Nugagahadamana Rd	LD/PO2/46/2	1+300-1+740		RHS/LHS	P	Road improvement	K.G.Chandrawathi Nugagahadamana,Giritale
	46	Hathamuna Bridge-Nugagahadamana Rd	LD/PO2/46/3	1+750-1+950		LHS	P	Road improvement	S.Kadeesha Umma M.L.Seeinul Abdehen J.Nabeesha Nugagahadamana,Giritale
	46	Hathamuna Bridge-Nugagahadamana Rd	LD/PO2/46/4	2+070-2+100		LHS	P	Road improvement	B.P.Sunil gamini
	46	Hathamuna Bridge-	LD/PO2/46/5	2+100-2+150		LHS	P	Road improvement	

PO2		Nugagahadamana Rd							
	46	Hathamuna Bridge-Nugagahadamana Rd	LD/PO2/46/6	2+340-2+400		RHS	P	Road improvement	D.D.Dhanushika Gayan Sampath Hatmuna,Hinguraggoda
	46	Hathamuna Bridge-Nugagahadamana Rd	LD/PO2/46/7	2+700-2+770		LHS/RHS	P	Road improvement	G.G.Somapala Hatmuna,Hinguraggoda
	46	Hathamuna Bridge-Nugagahadamana Rd	LD/PO2/46/8	2+950-3+000		LHS/RHS	P	Road improvement	S.M.Sandhaya kumara Hatmuna,Hinguraggoda
	46	Hathamuna Bridge-Nugagahadamana Rd	LD/PO2/46/9	3+190-3+220		LHS	P	Road improvement	H.M.Priyantha Pushpakumara Nugagahadamana,Giritale
	55	Hinguraka 4 th Mile postChandana Pokuna aliwanguwa Rd	LD/PO2/55/1	0+260-0+570		RHS	P	Road improvement	T.M.Jayasinghe Thennakoon H.R.M.Appuhamy Higuraka,Hinguraggoda
	55	Hinguraka 4 th Mile postChandana Pokuna aliwanguwa Rd	LD/PO2/55/2	0+700-0+960		RHS/LHS	P	Road improvement	T.M.N.Indu R.G.Yasawathi H.M.Ranmenike 4 th Mile post,Hinguraggoda
	55	Hinguraka 4 th Mile postChandana Pokuna aliwanguwa Rd	LD/PO2/55/3	0+750-0+850	0+750-0+850	RHS/LHS	P	Road improvement	A.L.M.Sulthan U.S.Jalaldheen Rishni Thiyas M.Sinulabdheen
	55	Hinguraka 4 th Mile postChandana Pokuna aliwanguwa Rd	LD/PO2/55/4	1+130-1+250		RHS	P	Road improvement	D.R.N.K.Dissanayaka Unagalawehera
	55	Hinguraka 4 th Mile postChandana Pokuna aliwanguwa Rd	LD/PO2/55/5	1+250-1+360		RHS/LHS	P	Road improvement	G.Silawathi Unagalawehera
	55	Hinguraka 4 th Mile postChandana Pokuna aliwanguwa Rd	LD/PO2/55/6	1+360-1+420		RHS	P	Road improvement	P.D.Mallika Track 8 Unagalawehera
	55	Hinguraka 4 th Mile postChandana	LD/PO2/55/7	1+870-3+130		RHS/LHS	P	Road improvement	R.P.Pushpalatha Track 8 Unagalawehera

PO2		Pokuna aliwanguwa Rd							
	55	Hinguraka 4 th Mile postChandana Pokuna aliwanguwa Rd	LD/PO2/55/8	3+200-3+400		RHS	P	Road improvement	H.M.Priyantha
	73	Chandhanapokuna Aliwanguwa Road	LD/PO2/73/05	0+933		RHS	P	Road improvement	
	29	Patunaagama junction dhamwalmankada bhuddagaya road	LD/PO2/29/01	0+000	0+040	LHS	P	Road improvement	A.D.G.Samantha bandara
	29	Patunaagama junction dhamwalmankada bhuddagaya road	LD/PO2/29/02	0+045	0+050	LHS	P	Road improvement	A.G.D.Dhanapala
	29	Patunaagama junction dhamwalmankada bhuddagaya road	LD/PO2/29/03	0+055	0+075	LHS	P	Road improvement	D.E.T.G.Kumara sri senarathna
	46	Hath amuna bridge - nugagahadamana road	LD/PO2/46/10	1+846	1+856	LHS		Road improvement	S.A.M.Kiyas
	46	Hath amuna bridge - nugagahadamana road	LD/PO2/46/11	1+807	1+825	RHS		Road improvement	M.S.Sinathul nisha
	46	Hath amuna bridge - nugagahadamana road	LD/PO2/46/12	1+760	1+784	RHS		Road improvement	M.A.Pashima
	46	Hath amuna bridge - nugagahadamana road	LD/PO2/46/13	1+831	1+860	RHS		Road improvement	Y.M.Sellima
	46	Hath amuna bridge - nugagahadamana road	LD/PO2/46/14	1+797	1+807	RHS		Road improvement	U.S.Jelaldheen
	46	Hath amuna bridge - nugagahadamana road	LD/PO2/46/15	1+784	1+797	RHS		Road improvement	M.B.M.Paisal
	46	Chandhanapokuna Aliwanguwa Road	LD/PO2/46/16	2+230	2+250	RHS		Road improvement	A.G.Rathnayaka
	46	Chandhanapokuna Aliwanguwa Road	LD/PO2/73/06	2+198	2+230	RHS		Road improvement	R.M.B.G.Dhumindha Niroshan
	46	Chandhanapokuna Aliwanguwa Road	LD/PO2/73/07	2+182	2+198	RHS		Road improvement	M.P.Sriyanoi malkanathi

PO2	46	Chandhanapokuna Aliwanguwa Road	LD/PO2/73/08	2+182	2+095	RHS		Road improvement	W.P.Babynona
	46	Chandhanapokuna Aliwanguwa Road	LD/PO2/73/09	2+082	2+095	RHS		Road improvement	S.E.N.A.Gabadage
	46	Chandhanapokuna Aliwanguwa Road	LD/PO2/73/10	2+055	2+019	RHS		Road improvement	H.M.Herath banda
	46	Medhirigiriya town internal road	LD/PO2/73/11	1+161	1+174	RHS		Road improvement	S.D.A.R.Boralessa
	73	Medhirigiriya town internal road	LD/PO2/40/01	1+161	1+174	RHS		Road improvement	M.G.D.I.Kumari
	40	Medhirigiriya town internal road	LD/PO2/40/02	1+070	1+110	RHS		Road improvement	R.M.Wasaantha manike
	31	Kumbukkanaruwa-127 Baudarthagama	LD/PO2/31/1	0+000-0+500		RHS	P	Road improvement	-
PO3	67	Aluthoya Village Rd	LD/po3/6701	0+315		LHS	P	Lead away	Chaminda Kumara 46, Althoya , Dimbulagala